

TO: City Plan Board

Item Number: 1.

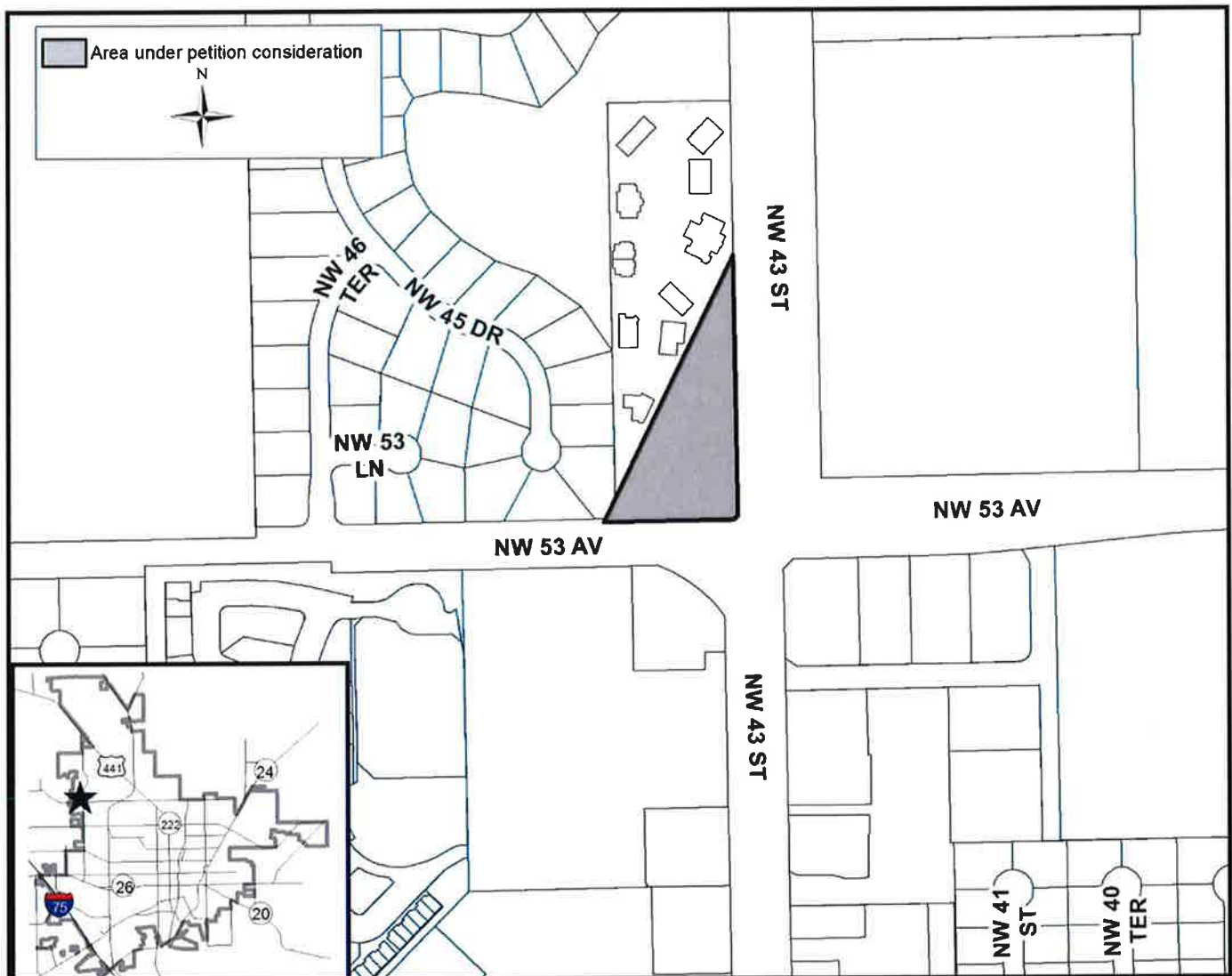
FROM: Planning & Development Services Department

DATE: June 23, 2016

SUBJECT: Petition PB-16-50 SUP: Boos Development Group/CHW, agent for Jeff Montgomery Associates, LLC, owner. Special use permit with development plan review for construction of a new commercial development with drive through. Zoned: MU-1 (8-30 units/acre mixed use low intensity). Located at 4322 NW 53rd Avenue.

Recommendation

Staff recommends approval of Petition PB-16-50 SUP with conditions and the related development plan.



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Description

This petition is a request for a special use permit at the above referenced site to allow construction of a retail and office development totaling 16,176 square feet of gross floor area, with three (3) drive through facilities. The site has a land use designation of Mixed Use Low (MUL) and a zoning of MU-1 (Mixed use low intensity 8-30 units per acre). Per the Comprehensive Plan, Policy 10.4.4 of the Transportation Mobility Element, new development of drive through facilities shall be based on compliance with the criteria for special use permits plus a set of criteria outlined in Policy 10.4.4.

The project site is located at the northwest corner of NW 43rd Street and NW 53rd Avenue on 2.69 acres, identified as Tax Parcel 10859-010-003. It entails demolition of an existing 11,000 square foot building that was used as a GRU Operation Center office. The building will be replaced with three buildings of approximately 16,176 square feet. The parcel has a right-angle, triangular shape along NW 53rd Avenue and NW 43rd Street, which presents some design and layout constraints to any development proposal on the site. The site is also within close proximity of a single-family residential neighborhood and has a common boundary with an office development along the western boundary of the site; a large shopping center exists across the street to the south and a public service facility to the east. Other uses within the area of the site includes an elementary school, single and multi-family residential, a shopping centers, a public park, places of religious assembly, financial institutions, and a retirement facility.

The development proposal includes a site plan showing the layout of the development with three free-standing buildings; two facing NW 43rd Street and the largest building, 8,900 square feet, fronting NW 53rd Avenue. The two smaller buildings fronting NW 43rd Street have drive-throughs and the larger medical building has two automobile drop-off and reception areas. Parking surrounds the main buildings along the roadways and all buildings are linked by a network of sidewalks connected to the main east-west/north-south sidewalks fronting the primary roadways. Other features of the site include landscaping, solid waste facilities and two primary access points onto the major road network.

One significant element of the development is the emergency medical facility and the use of ambulances with sirens servicing the facility. This is particularly relevant because of the relationship and proximity of the facility to single-family residential developments.

The project is seeking a general Special Use Permit and a Transportation Mobility Program Area (TMPA) Special Use Permit with preliminary and final development plan approval.

Key Issues

The issues pertaining to this development and its request for a Special Use permit and TMPA Special Use Permit are as follows:

1. The parcel has a unique and very constraining triangular shape.
2. It fronts two major east-west/north-south roadway arterials.
3. It has six automobile serving points associated with three buildings.
4. It is within close proximity to single-family residential development.
5. The development includes a use with the potential to use ambulances with sirens on a regular basis.
6. The development does not meet the building placement relative to both 43rd and 53rd arterials.
7. The width of the main sidewalks along both NW 43rd and NW 53rd does not meet the required minimum width of 8 feet.
8. The lighting plan shows excess spill-over lighting onto the adjacent property.

Basis for Recommendation

Staff's recommendation is based on the criteria for issuing a Special Use Permit as stated in the Land Development Code Section 30-233 and the requirements of the Transportation Mobility Program Area, Policy 10.4.4 of the Comprehensive Plan.

Special Use Permit Criteria

In accordance Section 30-233 no special use permit shall be approved by the City Plan Board unless the following findings are made concerning the proposed special use:

- 1. That the use or development complies with all required regulations and standards of this chapter and all other applicable regulations.***

The proposed development is in conformance with the land use designation of Mixed-Use Low and the proposed uses are consistent with the MU-1 zoning district. The development has been subject to development plan review which ensures compliance with required development standards. The attached comments and conditions of the Technical Review Committee are included to ensure that areas of potential deficiencies are addressed. The project is also subject to the appropriate Federal, State and local agency regulations. The applicant is requesting waivers from the following required standards; the build-to line, width of the sidewalk along the primary frontage roads, lighting spill-over of more than 1.0 fc onto the adjacent property, pole height of more than 15 feet, and the requirement to have an automatic shut-off after hours of operation. Staff supports the request for the waiver from the build-to line.

Condition 1. *The applicant shall be required to comply with all requirements of the Technical Review Committee as included in Attachment "A".*

- 2. That the proposed use or development will have general compatibility and harmony with the uses and structures on adjacent and nearby properties.***

The proposed development is in the MU-1 zoning district which is generally characterized as a mixed-use low intensity district established for the purpose of providing a mix of employment, retail, professional, service and residential uses in low level activity centers. It is also consistent with several of the listed objectives of the MU-1 district (See Attachment "D", Land Development Code reference). The general area can be characterized as a neighborhood activity center with a mixture of employment, retail, professional, recreational and educational services catering to the surrounding residential areas.

The subject site has a triangular shape which imposes a variety of constraints on the layout and design of the development relative to the surrounding uses. The buildings are positioned near the middle of the site with parking along the two frontage roads. This layout is consistent with the Central Corridor development standards of a double frontage row of parking along the primary arterials. The proposed uses are highly auto-oriented and automobile intensive; the bank has two auto serving points, the coffee shop has one and the medical facility has two, a total of five automobile serving points in addition to the regular trip ends associated with indoor services and the outdoor cafe. The development therefore may draw a high volume of frequent short term trips on a daily basis. This type of service requires efficient access to the site and a organized internal network of driveway aisles. The drive through lanes and automobile serving points are located to the rear of the buildings and designed primarily to integrate the need for all three buildings to accommodate the automobile and to facilitate internal circulation. The proposed layout design is also consistent with the design principle of screening drive-throughs from the public right-of-ways. However, the design forces the internal

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drive through activities, the solid waste pick-up, and the emergency ambulance drop-off, toward the office and residential areas along the west boundary of the development where they will have the potential to exert the greatest impact on the nearby uses.

Concerning this criteria related to “*general compatibility and harmony with the uses and structures on adjacent and nearby properties*”; *although this Special Use Permit is to address the drive through and not the medical use, both the development review criteria and the purpose and intent statement of the MU-1 zoning, requires maintenance of compatibility of all developments with surrounding uses. It is therefore relevant to address all potential compatibility issues related to the proposed development as indicted in the following references:*

The MU-1 states the following:

Sec. 30-64.

(a) Purpose

“..... *The district is intended to reduce the length and number of vehicular trips by **providing for basic needs within close proximity to residential areas, by encouraging pedestrian access, and by the combining of trips.** This district is established to allow uses compatible with each other and with surrounding residential areas to be developed near each other. The MU-1 district may be located in areas where analysis of residential characteristics demonstrates that such facilities are required. This district is intended to encourage the development of planned and unified neighborhood shopping centers **in a relationship harmonious with adjoining residential activities.** It is also intended to accommodate traditional neighborhoods that include nonresidential uses and neighborhood centers....”*

“....

(b) Objectives.....

- (5) *Ensure, through development plan approval, that nonresidential and mixed-use developments are designed to **promote the most efficient use of the land, and that they coordinate the internal activities of the site** as well as **establish a harmonious relationship between such developments and their environment.***
- (6) *Require **buffering or screening around nonresidential and/or mixed-use development** in accordance with the land development code when the development abuts any property zoned for residential use or shown as residential on the future land use map.....”*

Sec. 30-151. - Purpose.

*“The purpose of this division is to **promote harmonious, functional relationships among the various elements within any development** such as the location of activities, vehicular and pedestrian circulation systems, and visual form, **to ensure physical, social and economic compatibility with neighboring developments** and conditions, as well as with the community at large, and to **prevent detrimental impact to the natural environment on and off the site** by providing for review and evaluation of development plans for all development. Development plan review is intended to **permit maximum flexibility in reviewing each plan on its merits** and encourage variety and innovation within the intent and purpose specified for each zoning district and the minimum requirements specified in the Code of Ordinances.”*

Office uses are better able to sustain the relationship and impacts of drive throughs, thus the impact of the development on the office uses is less critical. Although more sensitive to non-residential uses, the residential development is sufficiently buffered both by distance and physical obstruction so that potential impacts from the drive-throughs will be minimized. On the other hand, other aspects of the development such as the medical facility with the use of ambulance with sirens, creates a potentially incompatibility relationship with the single-family residential and office uses. The same holds true of the height of light fixtures and the intensity of spill-over lighting onto the adjacent west properties. The code also requires that lighting shall be automatically

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extinguished no later than one hour after the close of business or facility operation and/or use by the public. This requirement is particularly important in light of the adjacent residential development and the high intensity levels required for the ATM services facing the west side of the property.

The lighting issues of the proposed development can be addressed by lowering the height of light fixtures to 15 feet and providing a solid screening mechanism to reduce the degree of light spill-over. Staff has provided conditions listed below to address the lighting concern.

Due to the triangular shape of the property and its location at the intersection of NW 43rd Street and NW 53rd Avenue, access to the site is forced to be along those roadways. The 43rd Street access is approximately 540 feet from the intersection and the 53rd Avenue access is about 300 feet from the intersection. The traffic engineer has determined that those distances allow adequate separation from the intersection to allow unrestricted access to the development without obstructions from traffic queued at the intersection awaiting signal changes. Both City and County transportation staff have expressed some concerns about the development's impact on school zone during specific times. The main issue appears to be left turns out of the development onto NW 53rd Avenue and NW 43rd Street during the hours of 7:30am to 8:30am and from 2:30pm to 3:00pm. Staff has proposed a condition to address the issue that has been used in other areas with this situation.

Condition 2. *A minimum eight-foot wide sidewalk shall be provided along the west side of NW 43rd Street and along the north side of NW 53rd Avenue.*

Condition 3. *Light fixtures within 50 feet of the west property boundary shall be reduced to 15 feet.*

Condition 4. *The photometric plan shall be amended to reduce spill-over lighting onto the adjacent property to levels allowed by the code. Alternatively, appropriate screening methods shall be employed to reduce the impact of the ATM lighting on the adjacent property. All light fixtures shall be designed and placed so that there is no direct lighting onto the adjacent neighborhoods. An automatic system shall be provided to lower lighting to security levels after operating hours.*

Condition 5. *Security safety lighting at a minimum of (0.5 foot candle) shall be provided along the sidewalk linking the adjacent development to the west.*

Condition 6. *There shall be no left turns onto NW 43rd Street from the development. The applicant shall add a no-left turn supplemental sign to the stop sign at NW 53rd Avenue restricting left turns during school hours for Talbot Elementary School.*

Condition 7. *The drive through communication at all five automobile service points shall be designed to prevent the transfer of such communications onto the adjacent residential areas.*

Condition 8.

Ambulance sirens shall only be used to enter and exit the site and not operated on site.

3. That necessary public utilities are available to the proposed site and have adequate capacity to service the proposed use and development.

The project has been reviewed by the Technical Review Committee and a determination has been made that the necessary public utilities are available to service the demands of the site. Potable water, sewer and wastewater systems are in place to provide adequate services. Although the demands for the new development may vary,

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the existing site is currently served by electric, gas, water, and sanitary sewer at a capacity which is adequate to serve the needs of the facility. GRU has recommended approval and indicates that there is adequate capacity to serve future needs as demand changes.

4. That the use or development is serviced by streets of adequate capacity to accommodate the traffic impacts of the proposed use.

The proposed development is located at the northwest corner of the intersection of NW 43rd Street and NW 53rd Avenue; both roadways are major arterials across the City and currently handle a large volume of traffic. Local uses have expressed concerns about the current traffic activities at the intersection and the need to facilitate pedestrian and bicycle circulation. Both roadways provide access to nearby local street networks as well as links to surrounding neighborhoods and communities. Both routes have adequate capacity to accommodate traffic associated with the development. The development is located in Zone B of the TMPA and must meet all requirements as stated in the Transportation Mobility Program Area Element of the Comprehensive Plan.

5. That screening and buffers are proposed of such type, dimension and character to improve compatibility and harmony of the proposed use and structure with the uses and structures of adjacent and nearby properties.

The proposed development satisfies the general landscaping and screening requirement per the Land Development Code; this is reflected in the proposed landscaping plan depicted in Attachment "B". Additional landscaping is proposed along the southwest boundary with the adjacent residential parcel. The northern 120 feet of the 43rd Street roadway frontage will not comply with the street buffer landscaping requirement. This is necessary to avoid conflict with existing and proposed underground utilities.

6. That the use or development conforms to the general plans of the city as embodied in the city comprehensive plan.

The property is zoned MU-1 which is consistent with the land use designation of Mixed Use low Intensity. The uses proposed for the property are uses allowed by right in the MU-1 zoning district, except that drive throughs are allowed by special use permit per the Comprehensive Plan. The proposed uses and development are therefore consistent with the intent of the Future Land Use Element of the Comprehensive Plan.

The project is classified as an infill/re-development project which is supported by Policy 2.1.1 of the Future Land Use Element of the Comprehensive Plan. The development can be described as a compact development which is supported by Objectives 1.5 and 2.1 of the comprehensive plan. The development is consistent with Policy 1.2.5 in the manner in which it addresses the needs of local pedestrians through the implementation of several sidewalk routes connecting residential neighborhoods to frequently used surrounding neighborhood activity centers.

7. That the proposed use or development meets the level of service standards adopted in the comprehensive plan and conforms with the concurrency management requirements of this chapter as specified in article III, division 2.

The development shall meet all level of service requirements. Staff has determined that the information provided is sufficient to justify compliance with the concurrency management requirements of the Comprehensive Plan. Per the TRC comments, prior to receiving a final development order, this development must sign a TMPA Agreement for the provision of eight (8) Transportation Mobility Element Policy 10.1.6 criteria.

In addition to the review criteria set in the Land Development Code for Special Use Permits, the approval of a drive-through facility shall be based on the following criteria:

Comprehensive Plan, Transportation Mobility Plan Areas (TMPA), Policy 10.4.4

The Comprehensive Plan classifies drive through facilities to include banking facilities, payment windows, restaurant, food and/or beverage sales, dry cleaning, express mail services, and other services that are extended mechanically or personally to customers who do not exit their vehicles. It requires that all such facilities demonstrate compliance with a specific set of criteria as listed below:

Policy 10.4.4 Criteria

The development is located within the MU-1 zoning district and the proposed uses are allowed by right, except for the drive throughs which are allowed through the special use permitting process. The development is proposing three (3) drive through serving points for the retail and financial intuition and two (2) automobile servicing points for the medical facility. In accordance with the above Comprehensive Plan policy, the following criteria are examined:

a. Maximization of pedestrian and bicycle safety and convenience;

The development proposal has provided for a well-connected internal system of sidewalks linking the main sidewalk routes along NW 43rd Street and NW 53rd Avenue. It also extends sidewalk connectivity to the adjacent office development which is a very positive aspect of the development. The existing sidewalks along NW 43rd Street and NW 53rd Avenue are 5 feet wide and not consistent with the 8-foot requirement of the code. The applicant is requesting a waiver to maintain the existing 5-foot sidewalk. Considering the surrounding uses of school, office, recreation, place of religious assembly, residential, shopping that will be associated with the development, there is a strong probability that sidewalks along the main arterials will be highly utilized and the code requirement for an 8-foot sidewalk will be needed.

b. Adequate queuing space for vehicles such that there is no back-up of traffic onto adjacent roadway

The development complies with the required queuing spaces; all the automobile serving points are provided with room for more than five queuing spaces. The design also ensures that vehicle stacking will occur on site. Staff expressed concerns about turning movements to access the dumpster which is located near the entrance off NW 43rd street. Modifications have been proposed to alleviate direct access to the dumpster from 43rd Street but the ideal situation would be to avoid the potential for conflicts at one of the main entrances to the facility.

Condition 9. The dumpster located near the 43rd Street entrance shall be relocated.

c. Provision of a by-pass lane or sufficient driveway area around the drive through lanes to assist internal vehicular circulation;

By-pass lanes are provided to all the automobile serving points proposed in the development.

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d. Minimization of the visual impacts of the drive through lanes on street frontage areas;

The drive-through lanes are position to the rear of the buildings thus minimizing visual exposure from the adjacent frontage streets. The most visible automobile serving point is located at the south east corner of the medical building.

e. Minimization of the total number of drive through lanes based on site conditions and the operating conditions of the impacted roadway segments;

Based on information from the applicant's traffic engineer, the design, number and configuration of drive throughs and automobile serving points on the site are based on business needs, internal circulation and the ability to accommodate safe and efficient movement of vehicles within the site. The main objective is to facilitate an efficient circulation of automobiles, free of conflicts with cyclist, pedestrians and other mode of travel. Most of the automobile serving points are located to the rear of the buildings and the accessing routes designed to integrate smoothly with the two main access points and primary parking areas. The external roadway designs and traffic circulation patterns were give strong consideration in designing the number of drive through lanes, their placement and the movement from service points to the ingress/egress points of the development.

f. Minimization of the number of access points to roadway;

The minimum of two (2) access points are allowed for the development; this would allow the site to function safely and efficiently.

g. Design of access points and ingress/egress directional flows to minimize impacts on the roadway and non-motorized traffic;

The design of access points and ingress/egress directional flows are minimized and is approved by staff.

h. Design of internal pedestrian access and safety as related to the position of the drive through lane(s); and;

The internal pedestrian routes are designed to minimize conflicts with the drive through lanes; the only pedestrian route crossing a drive through lane is the one providing connectivity to the adjacent office development to the west.

i. Meeting any additional design criteria established in the Land Development Code.

Approval of the design by the City's Traffic Engineer and the Alachua County Transportation engineer ensures that the development is in compliance with the requirements of the Land Development Code.

Condition 10. Because the development fronts two County Roads, NW 43rd Street and NW 53rd Avenue, approval from Alachua County Public Works must be obtained prior to final development plan approval from the City.

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Summary

Given the TRC comments and conditions of the staff report, the development will be in compliance with the development standards of the MU-1 zoning district as well as parking, access, landscaping and buffers requirements. Based on the above analysis staff has determined that the petition meets the criteria for issuing the TMPA Special Use Permit and the general Special Use Permit subject to the recommended conditions.

Respectfully submitted,



Ralph Hilliard

Planning Manager

Prepared by:



Lawrence Calderon

Lead Planner

Special Use Permit Conditions

Condition 1. *The applicant shall be required to comply with all requirements of the Technical Review Committee as included in Attachment "A".*

Condition 2. *A minimum eight-foot wide sidewalk shall be provided along the west side of NW 43rd Street and along the north side of NW 53rd Avenue.*

Condition 3. *Light fixtures within 50 feet of the west property boundary shall be reduced to 15 feet.*

Condition 4. *The photometric plan shall be amended to reduce spill-over lighting onto the adjacent property to levels allowed by the code. Alternatively, appropriate screening methods shall be employed to reduce the impact of the ATM lighting on the adjacent property. All light fixtures shall be designed and placed so that there is no direct lighting onto the adjacent neighborhoods. An automatic system shall be provided to lower lighting to security levels after operating hours.*

Condition 5. *Security safety lighting (.5 foot candle) shall be provided along the sidewalk linking the adjacent development to the west.*

Condition 6. *There shall be no left turns onto NW 43rd Street from the development. The applicant shall add a no-left turn supplemental sign to the stop sign for the development restricting left turns during school zones time for Talbot Elementary School.*

Condition 7. *The drive through communication at all five automobile service points shall be designed to prevent the transfer of such communications onto the adjacent residential areas.*

Condition 8.

Ambulance sirens shall only be used to enter and exit the site and not operated on site.

Condition 9. The dumpster located near the 43rd Street entrance shall be relocated.

Condition 10. Because the development fronts two County Roads, NW 43rd Street and NW 53rd Avenue, approval from Alachua County Public Works must be obtained prior to final development plan approval from the City.

List of Exhibits

Attachment A: Technical Review Committee Comments

Attachment B: Development Plans & Maps

Attachment C: Application and Neighborhood Workshop Information

Attachment D: Comprehensive Plan and Land Development Code References.

Attachment E: Applicant's Waiver Request and Response to Comments

Attachment "A"

TECHNICAL REVIEW COMMITTEE COMMENTS

PLANNING & DEVELOPMENT SERVICES DIVISION
 THOMAS CENTER BUILDING "B"
 306 NE 6TH AVENUE (352)334-5023

PETITION NO. <u>PB-16-50 SUP</u>	DATE PLAN RECEIVED: 05/17/2016	<input checked="" type="checkbox"/> Prelim Dev <input type="checkbox"/> Concept <input checked="" type="checkbox"/> Final Dev <input type="checkbox"/> Minor Dev. <input type="checkbox"/> Amendment <input type="checkbox"/> Minor Sub. <input type="checkbox"/> Special Use <input type="checkbox"/> Street <input type="checkbox"/> Planned <input type="checkbox"/> Vacation <input type="checkbox"/> Design Plat <input type="checkbox"/> Other: Land Use
REVIEWING BODY: Technical Review Cmt.	REVIEW DATE: 06/03/2016	
	REVIEW LEVEL: N/A	
PROJECT DESCRIPTION: <u>Petition PB-16-50 SUP</u> Boos Development Group/CHW, agent for Jeff Montgomery Associates, LLC, owner. Special use permit with major development plan review for the construction of a new commercial development with drive throughs. Zoned: MU-1 (8-30 units/acre mixed use low intensity). Located at 4322 NW 53 rd Avenue.	PROJECT PLANNER: Lawrence Calderon	
PROJECT LOCATION: Located at 4322 NW 53 rd Avenue.	PROPERTY AGENT: Boos Development Group/CHW, agent for Jeff Montgomery Associates, LLC, owner	

RECOMMENDATIONS/REQUIREMENTS/COMMENT

Planning Comments: Approvable subject to comments.
Lawrence Calderon, Lead Planner,
352-334-5023 June 10, 2016

1. Please ensure that all uses proposed for the development are allowable uses within the MU-1 district. *Outdoor storage and sales are not allowed within any business district.*
2. The Development does not meet the build-to line; a waiver of the build-to line from 80 feet to 100 feet is requested and subject to board approval.
3. The required sidewalk width along arterials is 8 feet; the existing sidewalks along 43rd and 53rd appear to be less than the required width. The Special Use Permit is to allow a drive-thru in accordance with

TRC COMMENTS

(CONTINUED)

Policy 10.4.5 of the Comprehensive Plan which requires that pedestrian and bicycle circulation be adequately addressed. It is therefore anticipated that given the new development, the school, office uses and the shopping center, the sidewalk should meet the code requirements.

4. The dumpster at the entrance of the site from NW 43rd may not be the best location; please consider an alternate location. The concern is about restricting circulation at the 43rd Street entrance.
5. Details for the outdoor seating between the two buildings have not been presented. Outdoor activities shall be limited to what is allowed by the Land Development Code. Alcohol for consumption on premises may be included with the development only if the outdoor cafe is accessory to an eating establishment in compliance with the 51% rule.
6. The code requires the following:
 - a. Lighting shall be automatically extinguished no later than one hour after the close of business or facility operation and/or use by the public; I did not see how this is addressed.
 - b. The spill-over lighting onto the adjacent property to the west can be avoided; please address.
 - c. Staff recommends landscaping, fencing, screening or other lighting modifications to address the spill-over lighting in the area.
7. It appears that the medical development will handle emergencies involving sirens to and from the facility. Given the high volume of traffic activities in the area, emergency vehicles may have the need to use sirens more frequent, leading to conflict and nuisance with the adjacent residential and office uses. This issue has not been addressed.

Fire and Life Safety Services (Approvable) **Steve Hesson, Fire Inspector, 334-5065**

Fire and Life Safety Services (approvable)
Tom Burgett, Fire Inspector, 334-5065

1. Please indicate the approximate location of the features of fire protection, including:
--Fire department connection: Fire department connections shall be located at the nearest point of fire department apparatus accessibility or at a location approved by the authority having jurisdiction.
(NFPA 13- 8.17.2.4.6)

[Gainesville Land Development Code Section 30-157(d) (15)]

Email sent to firm about the FDC

GRU Comments (Waiting for comments as of 6-10-2016.)
Neal Beery, GRU New Services, 352-393-1413, newservices@gru.com

GRU comments are being provided to the applicant in the form of redline markups of the plans.

Real Estate: (Not Approved)

No Comments

Water/WasterWater: (Not Approved)

No Comments

TRC COMMENTS
(CONTINUED)

Electric: (Approved)

No Comments

GRUCom: (Approved)

No Comments

GRU GAS: (Approved)

No Comments

Environmental Comments: No Comments

Mark Brown, Environmental Coordinator

E-mail: hendrixjw@cityofgainesville.org; Phone: 352-393-8

Concurrency Comments (Approvable subject to below)

Jason Simmons, Concurrency Planning, 334-5022

1. Prior to receiving a final development order, this development must sign a TMPA Agreement for the provision of 8 Transportation Mobility Element Policy 10.1.6 criteria. The payment to be made to meet will go towards meeting the criteria as listed in Policy 10.1.6.

Building Department Comment: No comments

Linda Patrick, Plans Examiner (PX934), 334-5050

patricklr@cityofgainesville.org

Urban Forestry Comments (Approvable as submitted)

Earline Luhrman, Urban Forestry Inspector, 393-8188

5/25/16

1. Approvable as submitted.

Public Works Department Comments (APPROVABLE - Subject to Comments)

Public Works Review (Approvable subject to comments)

352-334-5070

Stormwater Management:

1 - For SMF-1, it appears the structure S-1 would be the lowest inlet top associated with this facility. Currently, the calculations show that .09 feet of freeboard is provided within the stormwater facility. The additional storage can be provided in the proposed parking lot, but S-1 would drain directly into the right-of-way.

TRC COMMENTS

(CONTINUED)

Hazardous Materials - ACEPD (No Comments)

Agustin Olmos, Water Resources Supervisor, PE, 264-6800

GPD Crime Prevention Unit Comments: No Comments

Dr. Richard Schneider, Title: 334-2385;

GPD Crime Prevention Unit Comments

ATTACHMENT "B"

Development Plan Maps and Drawings

DEVELOPMENT PLANS FOR

43RD STREET AND 53RD AVENUE RETAIL DEVELOPMENT

GAINESVILLE, FLORIDA

SECTION 15, TOWNSHIP 9 SOUTH, RANGE 19 EAST

SUBMITTED TO
CITY OF GAINESVILLE

ALACHUA COUNTY

GAINESVILLE REGIONAL UTILITIES

ST. JOHNS RIVER WATER MANAGEMENT DISTRICT

DEVELOPER

BOOS DEVELOPMENT GROUP, INC
410 PARK PLACE BLVD.,
SUITE 100
CLEARWATER, FL 33759
(727) 669-2000
info@boosdevelopment.com

CIVIL ENGINEER

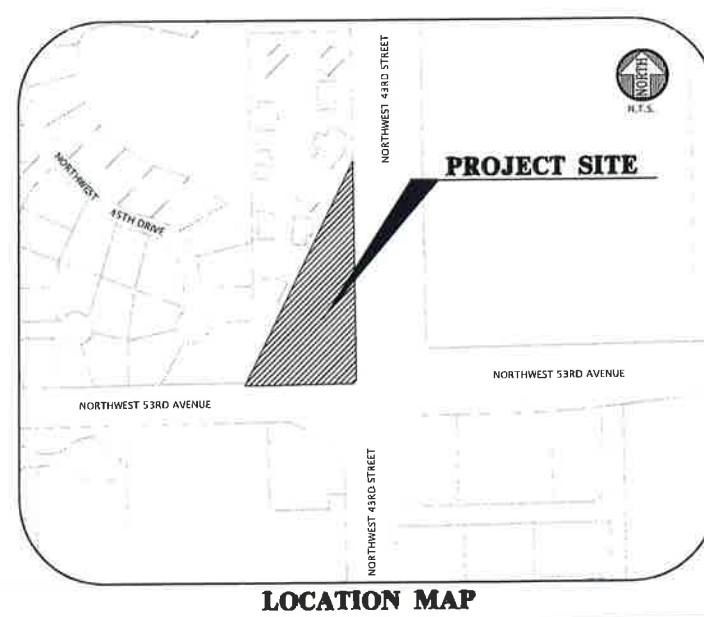
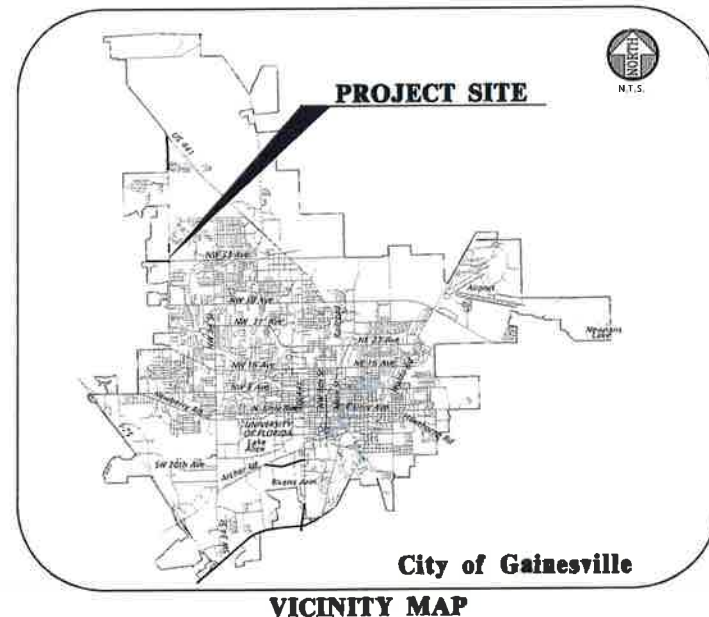
ROBERT J. WALPOLE, P.E.
CAUSSEAUX, HEWETT, & WALPOLE, INC.
132 N.W. 76th DRIVE
GAINESVILLE, FLORIDA 32607
(352) 331-1976
walpole@chw-inc.com

LANDSCAPE ARCHITECT

BURFORD DAVIS, ASLA
BURFORD DAVIS & ASSOCIATES
2406 NW 43RD STREET
GAINESVILLE, FLORIDA 32606
(352) 335-1896

PHOTOMETRIC CONSULTANT

RONALD J. NIX, P.E.
NIX ENGINEERING, INC.
2711 NW 6TH STREET SUITE B
GAINESVILLE, FLORIDA 32609
(352) 271-9900



1. DEVELOPMENT DATA:

EXISTING DATA:			
TOTAL AREA=	116,980 SF	100.0%	2.69 ACRES
TOTAL IMPERVIOUS AREA=	0 SF	0%	0 ACRES
OPEN AREA=	116,980 SF	100.0%	2.69 ACRES
PROPOSED DATA:			
TOTAL AREA=	116,980 SF	100.0%	2.69 ACRES
TOTAL IMPERVIOUS AREA=	84,925 SF	72.6%	1.93 ACRES
OPEN AREA=	32,055 SF	27.4%	0.75 ACRES

DESCRIPTION:
THE PROPOSED DEVELOPMENT IS A 2.5, 100 SF OF COMMERCIAL RETAIL DEVELOPMENT AND 10,820 SF OF MEDICAL USE WITH ASSOCIATED HARDSCAPE, PARKING, UTILITIES, AND STORM WATER FACILITIES. THE PROJECT SITE IS LOCATED AT THE NW CORNER OF NW 43RD STREET AND NW 53RD AVENUE, GAINESVILLE, FLORIDA. THE PROJECT SITE TOTAL AREA IS APPROXIMATELY 2.69 ACRES.

2. SITE ZONING: MIXED USE - 1 (MU-1)

LAND USE:
MIXED USE (1) (MU1)

BUILD-TO-LINE:

LOCATION	REQUIRED	PROVIDED
FRONT (NW 43RD STREET)	BUILDING FACADE SHALL BE NO CLOSER THAN 20' FROM CURB OR EDGE OF PAVEMENT	92' ±
FRONT (NW 53RD AVENUE)	BUILDING FACADE SHALL BE NO CLOSER THAN 20' FROM CURB OR EDGE OF PAVEMENT	96' ±

BUFFERS: N/A

3. PARKING:

REQUIRED:			
VEHICULAR PARKING:			= 110 SPACES
COFFEE SHOP = 3 SPACES PLUS 1 PER 3 SEATS =		3 + 75/3 = 28	= 28 SPACES
BANK = 1 PER 300 SF =		3,028/300 = 10.1	= 10 SPACES
MEDICAL = 1 PER 150 SF =		10,848/150 = 72.3	= 72 SPACES
BIKE: 10 PERCENT OF REQUIRED VEHICULAR PARKING =		110 X 0.10 = 11 SPACES	BIKE RACKS

PROVIDED:
VEHICULAR PARKING: 110
BIKE: 6

4. UTILITIES:

ALL UTILITY SERVICES SHALL BE INSTALLED BELOW GRADE PER LOC SECTION 30-345.
WATER: - NEW DOMESTIC WATER, IRRIGATION, AND FIRE SUPPRESSION LINE TO BE CONNECTED TO EXISTING WATER LINE ON NW 53RD AVENUE.

WASTEWATER: - CONNECT VIA EXISTING SANITARY LINE NW 53RD AVENUE.

ELECTRIC: - NEW ELECTRIC TO BE CONNECTED TO EQUIPMENT ON NW 43RD STREET.

GAS: - GAS IS PROPOSED TO BE CONNECTED TO AN EXISTING UNDER NW 53RD AVENUE.

5. STORMWATER MANAGEMENT UTILITY DATA:

A STORMWATER SYSTEM WILL BE CONSTRUCTED ON THE SITE.
TOTAL IMPERVIOUS AREA = 84,925 S.F.

	SMP-1 (WQ75)	SMP-1 (WQ75)
LOWEST DISCHARGE ELEVATION	7.712	7.733
RETENTION VOLUME BELOW LOWEST DISCHARGE ELEVATION	7,142 C.F.	6,258 C.F.
RETENTION AREA AT LOWEST DISCHARGE ELEVATION	11,712 S.F.	11,710 S.F.

THE PROJECT MUST COMPLY WITH ALL NPDES CRITERIA BOTH DURING AND AFTER CONSTRUCTION.

6. REFUSE COLLECTION: INTERIOR TRASH COLLECTION. REFER TO ARCHITECTURAL PLANS FOR DETAILS.

7. SITE LIGHTING IS PROVIDED. THE LIGHT LOCATIONS PROVIDE FULL CUTOFF LUMINARIES AND COMPLIES WITH ARTICLE VI OF THE LAND DEVELOPMENT CODE. REFER TO PHOTOMETRIC DESIGN FOR DETAIL.

8. ALL NEW TRAFFIC CONTROL DEVICES (SIGNS AND PAVEMENT MARKINGS) SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND FLORIDA D.O.T. STANDARDS.

9. FIRE PROTECTION:

THE BUILDING SHALL COMPLY WITH THE FLORIDA FIRE PREVENTION CODE. FIRE PROTECTION SYSTEMS PROVIDED IN THE STRUCTURES SHALL BE INSTALLED IN FULL COMPLIANCE WITH THE APPROPRIATE FIRE PROTECTION AND BUILDING CONSTRUCTION STANDARDS. ALL STABILIZED SURFACES MUST BE IN PLACE PRIOR TO ANY ACCUMULATION OF COMBUSTIBLES ON SITE.
THE BUILDING IS SPRINKLED. IN-BUILDING PUBLIC SAFETY RADIO ENHANCEMENT SYSTEMS SHALL BE PROVIDED IN ALL NEW BUILDINGS WHERE MINIMUM RADIO SIGNAL STRENGTH FOR FIRE DEPARTMENT COMMUNICATIONS IS NOT ACHIEVED AT A LEVEL DETERMINED BY THE AFD.

THE OWNER OR THE OWNER'S AUTHORIZED AGENT SHALL DEVELOP A FIRE SAFETY PROGRAM TO ADDRESS ALL ESSENTIAL FIRE AND LIFE SAFETY REQUIREMENTS FOR THE DURATION OF DEMOLITION, ALTERATION AND CONSTRUCTION, AS SPECIFIED IN THE FLORIDA FIRE PREVENTION CODE, INCLUDING NFPA 781. THE FIRE SAFETY PROGRAM SHALL INCLUDE AN EMERGENCY RESPONSE PLAN, AS WELL AS IDENTIFYING FIRE PREVENTION MEASURES, SITE AND BUILDING EMERGENCY ACCESS ROUTES, TEMPORARY AND PERMANENT WATER SUPPLIES, BUILDING EGRESS ROUTES, GOOD HOUSEKEEPING PRACTICES, AND FIRE PROTECTION SYSTEM INSTALLATION AND MAINTENANCE. (GAINESVILLE FIRE PREVENTION AND PROTECTION CODE SECTION 10-9 (NFPA 1-18))

GENERAL NOTES

- NO GENERAL HAZARDS ARE PROPOSED FOR THIS PROJECT. N/A
- THERE ARE NO KNOWN SPECIAL FIRE PROTECTION CONCERNS. N/A
- THE SITE IS NOT IN THE HISTORICAL PRESERVATION DISTRICT. N/A
- THE SITE IS NOT IN A FLOOD PLAIN DISTRICT. N/A
- THE SITE IS NOT EFFECTED BY WETLANDS REGULATIONS. N/A
- THE SITE IS NOT LOCATED ON A NATURE PARK, GREENWAY, OR GATEWAY DISTRICT. N/A
- THE SITE IS NOT LOCATED WITHIN THE WELLFIELDS PROTECTION DISTRICT. N/A
- THE MAXIMUM HEIGHT OF THE PROPOSED STRUCTURE IS LESS THAN 90 FEET. PER APPENDIX F SECTION II.A.2.B, THIS STRUCTURE IS WITHIN SUBZONE 1, GREATER THAN 15,000 FEET FROM THE AIRPORT, IT IS LESS THAN 100 FEET TALL, AND IS THEREFORE NOT TO BE CONSIDERED A "POTENTIAL AIRPORT OBSTRUCTION".

- THE SITE COMPLIES WITH THE FLORIDA BUILDING CODE FIFTH EDITION AND ALL RELATED CODES AND FLORIDA HANDICAPPED ACCESSIBILITY CODES AND STANDARDS INCLUDING THE FOLLOWING:
FLORIDA BUILDING CODE - BUILDING FIFTH EDITION
FLORIDA BUILDING CODE - EXISTING FIFTH EDITION
FLORIDA BUILDING CODE - RESIDENTIAL FIFTH EDITION
FLORIDA BUILDING CODE - PLUMBING FIFTH EDITION
FLORIDA BUILDING CODE - FUEL GAS FIFTH EDITION
FLORIDA BUILDING CODE - MECHANICAL FIFTH EDITION
FLORIDA BUILDING CODE - ENERGY CONSERVATION FIFTH EDITION
FLORIDA BUILDING CODE - ACCESSIBILITY FIFTH EDITION
NATIONAL ELECTRICAL CODE 2008 EDITION
NFPA 101 LIFE SAFETY CODE W/ FLORIDA MODIFICATIONS 2009 EDITION
FLORIDA FIRE PREVENTION CODE FIFTH EDITION.

- TRAFFIC STATEMENT:
THE PROJECT TRAFFIC GENERATION IS CALCULATED FROM TRIP GENERATION RATES PUBLISHED IN THE INSTITUTE OF TRANSPORTATION ENGINEERS (ITE) TRIP GENERATION, AN INFORMATIONAL REPORT, 9TH EDITION.

	EXISTING LAND USE	PROPOSED LAND USE	DRIVE IN BANK (D1)	COFFEE SHOP WITH DRIVE THRU	NET TOTAL
QUANTITY	12,400 SF	10,820 SF	2,729 SF	2,300 SF	2,036
ADT	269	143	279	1,883	224
AM PEAK HR	36	10	19	231	82
PM PEAK HR	92	10	66	98	

THE DEVELOPMENT IS LOCATED IN ZONE B OF THE TRANSPORTATION MOBILITY ELEMENT PROGRAM AREA (TMPA), AND SHALL MEET ALL CRITERIA AS LISTED IN THE TMPA POLICY 10.1.4 AND 10.1.6 CRITERIA. THIS DEVELOPMENT MUST MEET 8 TRANSPORTATION MOBILITY ELEMENT POLICY 10.1.6 CRITERIA. PRIOR TO RECEIVING A FINAL DEVELOPMENT ORDER, THIS DEVELOPMENT MUST SIGN A TMPA AGREEMENT FOR THE PROVISION OF 8 TRANSPORTATION MOBILITY ELEMENT POLICY 10.1.6 CRITERIA.

20. PARCEL INFORMATION:

TAX PARCEL # 08023 001 000

21. BUILDING OCCUPANCY CLASS: STARBUCKS: A-2; CHASE BANK: B; REMOTE ED: MEDICAL

22. BUILDING CONSTRUCTION TYPE: PER FBC-FIFTH EDITION, STARBUCKS: TYPE VB, CHASE BANK: TYPE VB, REMOTE ED: TYPE III

	STARBUCKS	CHASE BANK	REMOTE ED
23. BUILDING AREA TABULATION:			
PER FBC	2,214 SF	2,763 SF	10,848 SF
GFA	2,200 SF	3,028 SF	

24. BUILDING HEIGHT (PER FBC 2015): STARBUCKS: 17' 4"; CHASE BANK: 26' 4 1/2"; REMOTE ED: 27' 8"

25. PROPOSED USE:
THE ON-SITE BUILDINGS WILL CONSIST OF COMMERCIAL RETAIL AND MEDICAL USE.

SHEET INDEX

SHEET NUMBER	DESCRIPTION
CO.00	COVER SHEET AND INDEX
CO.10	GENERAL NOTES
CO.11	LEGEND
CO.12	CONSTRUCTION DETAILS
1 OF 2	TOPOGRAPHIC AND BOUNDARY SURVEY
CO.20	STORMWATER POLLUTION PREVENTION NOTES
CO.21	STORMWATER POLLUTION PREVENTION PLAN AND DETAILS
CO.30	DEMOLITION PLAN
C1.00	DEVELOPMENT PLAN AND HORIZONTAL CONTROL AND SITE PLAN
C1.01 - C1.02	DETAILED HORIZONTAL CONTROL AND SITE PLAN
C1.20	ACCESSIBILITY SITE PLAN AND DETAILS
C2.00	DRAINAGE AND GRADING PLAN
C2.01 - C2.02	DETAILED DRAINAGE AND GRADING PLAN
C2.30 - C2.31	STORMWATER MANAGEMENT FACILITY PLAN AND DETAILS
C3.00	UTILITY PLAN
LST - L52	LANDSCAPE PLANS
E1	PHOTOMETRIC PLAN
A0.03	STARBUCKS KEY NOTE SCHEDULE
A1.0 - A1.1	BUILDING FLOOR PLAN (STARBUCKS)
A3.0 - A3.1	BUILDING ELEVATIONS (STARBUCKS)
A1.0	BUILDING TRASH ENCLOSURE (STARBUCKS)
1 OF 1	BUILDING FLOOR PLAN (CHASE BANK)
1 OF 1	BUILDING ELEVATIONS (CHASE BANK)
1 OF 1	BUILDING ROOF PLAN (CHASE BANK)
1 OF 1	BUILDING TRASH ENCLOSURE (CHASE BANK)
A201	BUILDING ELEVATIONS (REMOTE ED)



FOR REVIEW ONLY CERTIFICATION

THE WATER & WASTEWATER SYSTEM DESIGN IS IN ACCORDANCE WITH CURRENT GRU DESIGN STANDARDS.
ROBERT J. WALPOLE
FL PE No. 58206

DATE: _____

GAS AND ELECTRIC DESIGN PROVIDED BY GRU ENERGY DELIVERY.

15-0490

FL PE No. 58206
SHEET NO. **CO.00**

PB-16-00050

DATE: 04/05/16
05/17/16

CLIENT: BOOS DEVELOPMENT GROUP
PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
SHEET TITLE: COVER SHEET AND INDEX

DESIGNER: J. THOMAS
CHECKER: R. WALPOLE
QUALITY CONTROL: R. SIMON
DATE: 04/05/16

DESIGNED BY: ROBERT J. WALPOLE

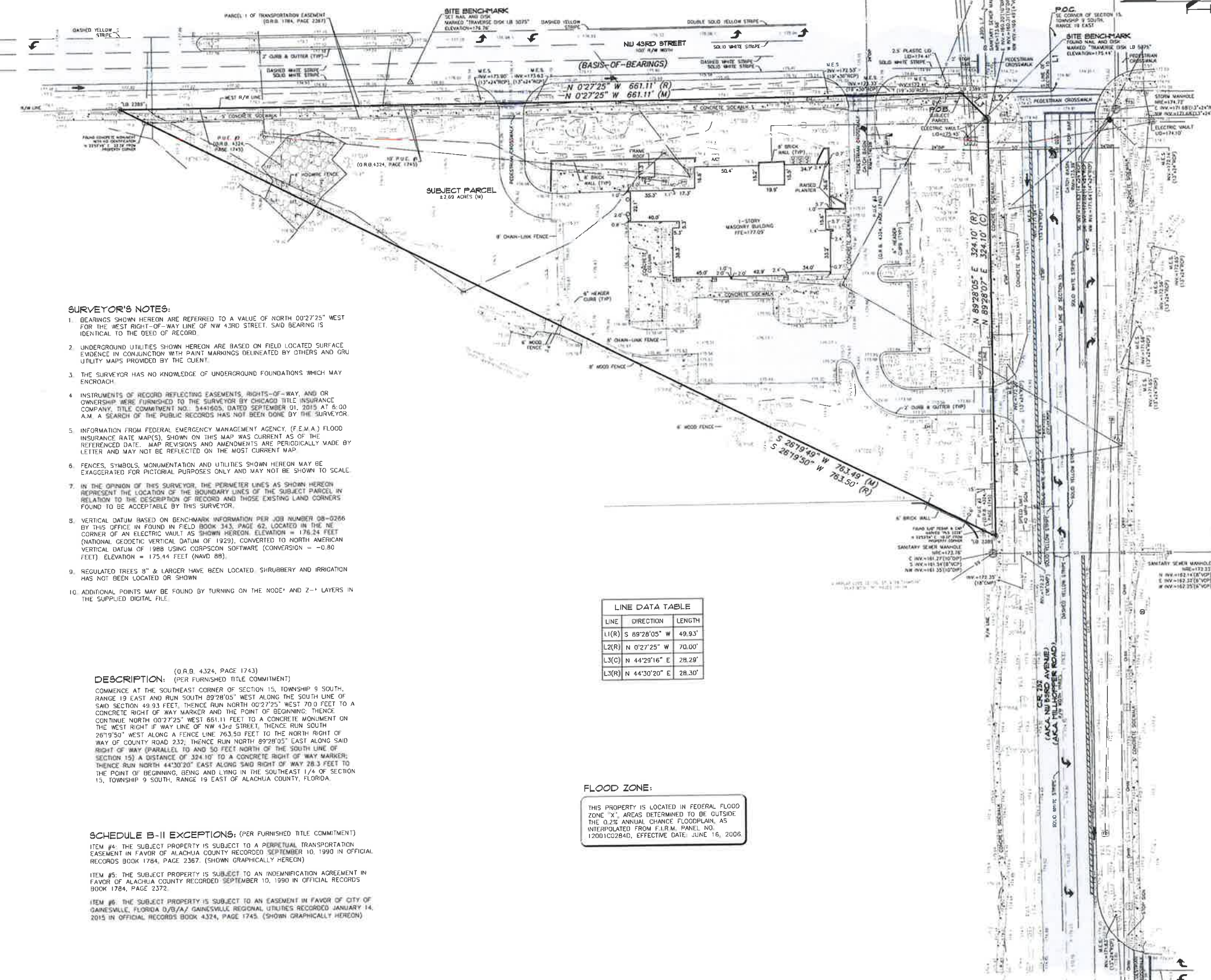
SCALE: N/A
VERTICAL SCALE: AS SHOWN
HORIZONTAL SCALE: AS SHOWN
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BOUNDARY & TOPOGRAPHIC SURVEY

LOCATED IN SECTION 15, TOWNSHIP 9 SOUTH, RANGE 19 EAST, ALACHUA COUNTY, FLORIDA

- LEGEND:**
- = FOUND 4"x4" CONCRETE MONUMENT
 - = FOUND NAIL AND DISK
 - = FOUND 3/8" STEEL REBAR & CAP MARKED AS SHOWN
 - ⊠ = BACKFLOW PREVENTOR
 - ⊠ = CONCRETE LIGHT POLE
 - ⊠ = CONCRETE POWER POLE
 - ⊠ = CABLE TELEVISION BOX
 - ⊠ = ELECTRIC HANDHOLE
 - ⊠ = ELECTRIC METER
 - ⊠ = FIRE HYDRANT
 - ⊠ = FIBER OPTIC BOX
 - ⊠ = CROSSWALK INDICATOR POLE
 - ⊠ = GUY ANCHOR
 - ⊠ = MAILBOX
 - ⊠ = SIGN
 - ⊠ = TREE (SIZE AND TYPE AS NOTED)
 - ⊠ = SANITARY SEWER MANHOLE
 - ⊠ = STORM SEWER MANHOLE
 - ⊠ = TELEPHONE JUNCTION BOX
 - ⊠ = TELEPHONE PEDESTAL
 - ⊠ = TRANSFORMER
 - ⊠ = TRAFFIC SIGNAL POLE
 - ⊠ = FIBERGLASS LIGHT POLE
 - ⊠ = WATER METER
 - ⊠ = WATER VALVE
 - ⊠ = STORM INLET
 - ⊠ = BOLLARD
 - ⊠ = SANITARY SEWER CONCRETE MARKER
 - ⊠ = ROOF DRAIN
 - X 175.5 = SPOT ELEVATION (PERVIOUS SURFACE)
 - X 175.75 = SPOT ELEVATION (IMPERVIOUS SURFACE)
 - (R) = DATA BASED ON DEED OF RECORD (O.R.B. 4324, PAGE 1743)
 - (M) = DATA BASED ON FIELD MEASUREMENTS
 - (C) = CALCULATED DATA
 - P.U.E. = PUBLIC UTILITIES EASEMENT
 - P.O.B. = POINT OF BEGINNING
 - P.O.C. = POINT OF COMMENCEMENT
 - O.R.B. = OFFICIAL RECORDS BOOK
 - TYP. = TYPICAL
 - R/W = RIGHT OF WAY
 - F.F.E. = FINISHED FLOOR ELEVATION
 - M.E.S. = MITERED END SECTION
 - A/C = AIR CONDITIONER
 - M.P.H. = MILES PER HOUR
 - C.R. = COUNTY ROAD
 - A.K.A. = ALSO KNOWN AS
 - ID = IDENTIFICATION
 - L.F. = LINEAR FEET
 - N.R.E. = NORTH RIM ELEVATION
 - I.W. = INVERT
 - R.C.P. = REINFORCED CONCRETE PIPE
 - P.V.C. = POLYVINYL CHLORIDE
 - C.M.P. = CORRUGATED METAL PIPE
 - U.I.P. = UJIC/ILL INGN PIPE
 - V.C.P. = VITRIFIED CLAY PIPE
 - G.S.P. = GALVANIZED STEEL PIPE
 - O.P. = CAST IRON PIPE
 - NAVD = NORTH AMERICAN VERTICAL DATUM
 - LO = LIVE OAK
 - LAO = LAUREL OAK
 - MAP = MAPLE
 - WO = WATER OAK
 - MYR = MYRTLE
 - MAD = MAGNOLIA
 - CHY = CHERRY
 - CED = CEDAR
 - = UNDERGROUND FIBER OPTIC LINE (PER GRU MAPS)
 - = UNDERGROUND SANITARY SEWER LINE (PER GRU MAPS)
 - = UNDERGROUND ELECTRIC LINE (PER GRU MAPS)
 - = UNDERGROUND TELEPHONE LINE (PER GRU MAPS)
 - = UNDERGROUND GAS LINE (PER GRU MAPS)
 - = ABANDONED UNDERGROUND GAS LINE (PER GRU MAPS)
 - = UNDERGROUND WATER LINE (PER GRU MAPS)
 - = FENCE (SIZE AND TYPE AS NOTED)
 - = OVERHEAD WIRE
 - SS = SANITARY SEWER LINE
 - ST = STORM SEWER LINE
 - = CONTOUR LINE
 - 175 = END OF FEATURE NOT DETERMINED
 - = ASPHALT SURFACE
 - = CONCRETE SURFACE
 - = GRAVEL SURFACE
 - = METAL LID



- SURVEYOR'S NOTES:**
- BEARINGS SHOWN HEREON ARE REFERRED TO A VALUE OF NORTH 00°27'25" WEST FOR THE WEST RIGHT-OF-WAY LINE OF NW 43RD STREET. SAID BEARING IS IDENTICAL TO THE DEED OF RECORD.
 - UNDERGROUND UTILITIES SHOWN HEREON ARE BASED ON FIELD LOCATED SURFACE EVIDENCE IN CONJUNCTION WITH PAINT MARKINGS DELINEATED BY OTHERS AND GRU UTILITY MAPS PROVIDED BY THE CUENT.
 - THE SURVEYOR HAS NO KNOWLEDGE OF UNDERGROUND FOUNDATIONS WHICH MAY ENCRDACH.
 - INSTRUMENTS OF RECORD REFLECTING EASEMENTS, RIGHTS-OF-WAY, AND OR OWNERSHIP WERE FURNISHED TO THE SURVEYOR BY CHICAGO TITLE INSURANCE COMPANY, TITLE COMMITMENT NO. 3441605, DATED SEPTEMBER 01, 2015 AT 8:00 A.M. A SEARCH OF THE PUBLIC RECORDS HAS NOT BEEN DONE BY THE SURVEYOR.
 - INFORMATION FROM FEDERAL EMERGENCY MANAGEMENT AGENCY (F.E.M.A) FLOOD INSURANCE RATE MAPS, SHOWN ON THIS MAP WAS CURRENT AS OF THE REFERENCED DATE. MAP REVISIONS AND AMENDMENTS ARE PERIODICALLY MADE BY LETTER AND MAY NOT BE REFLECTED ON THE MOST CURRENT MAP.
 - FENCES, SYMBOLS, MONUMENTATION AND UTILITIES SHOWN HEREON MAY BE EXAGGERATED FOR PICTORIAL PURPOSES ONLY AND MAY NOT BE SHOWN TO SCALE.
 - IN THE OPINION OF THIS SURVEYOR, THE PERIMETER LINES AS SHOWN HEREON REPRESENT THE LOCATION OF THE BOUNDARY LINES OF THE SUBJECT PARCEL IN RELATION TO THE DESCRIPTION OF RECORD AND THOSE EXISTING LAND CORNERS FOUND TO BE ACCEPTABLE BY THIS SURVEYOR.
 - VERTICAL DATUM BASED ON BENCHMARK INFORMATION PER JOB NUMBER 08-0266 BY THIS OFFICE IN FOUND IN FIELD BOOK 343, PAGE 62, LOCATED IN THE NE CORNER OF AN ELECTRIC VAULT AS SHOWN HEREON. ELEVATION = 176.24 FEET (NATIONAL GEODETIC VERTICAL DATUM OF 1929), CONVERTED TO NORTH AMERICAN VERTICAL DATUM OF 1988 USING CORPSCON SOFTWARE (CONVERSION = -0.80 FEET) ELEVATION = 175.44 FEET (NAVD 88).
 - REGULATED TREES 8" & LARGER HAVE BEEN LOCATED. SHRUBBERY AND IRRIGATION HAS NOT BEEN LOCATED OR SHOWN.
 - ADDITIONAL POINTS MAY BE FOUND BY TURNING ON THE NODE+ AND Z-1 LAYERS IN THE SUPPLIED DIGITAL FILE.

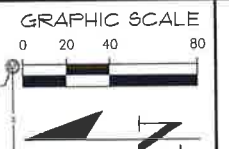
LINE DATA TABLE

LINE	DIRECTION	LENGTH
L1(R)	S 89°28'05" W	49.93'
L2(R)	N 0°27'25" W	70.00'
L3(C)	N 44°29'16" E	28.29'
L3(R)	N 44°30'20" E	28.30'

DESCRIPTION: (O.R.B. 4324, PAGE 1743)
 (PER FURNISHED TITLE COMMITMENT)
 COMMENCE AT THE SOUTHEAST CORNER OF SECTION 15, TOWNSHIP 9 SOUTH, RANGE 19 EAST AND RUN SOUTH 89°28'05" WEST ALONG THE SOUTH LINE OF SAID SECTION 49.93 FEET, THENCE RUN NORTH 00°27'25" WEST 70.0 FEET TO A CONCRETE RIGHT OF WAY MARKER AND THE POINT OF BEGINNING; THENCE CONTINUE NORTH 00°27'25" WEST 661.11 FEET TO A CONCRETE MONUMENT ON THE WEST RIGHT OF WAY LINE OF NW 43RD STREET; THENCE RUN SOUTH 26°19'50" WEST ALONG A FENCE LINE 763.50 FEET TO THE NORTH RIGHT OF WAY OF COUNTY ROAD 232; THENCE RUN NORTH 89°28'05" EAST ALONG SAID RIGHT OF WAY (PARALLEL TO AND 50 FEET NORTH OF THE SOUTH LINE OF SECTION 15) A DISTANCE OF 324.10' TO A CONCRETE RIGHT OF WAY MARKER; THENCE RUN NORTH 44°30'20" EAST ALONG SAID RIGHT OF WAY 28.3 FEET TO THE POINT OF BEGINNING, BEING AND LYING IN THE SOUTHEAST 1/4 OF SECTION 15, TOWNSHIP 9 SOUTH, RANGE 19 EAST OF ALACHUA COUNTY, FLORIDA.

SCHEDULE B-II EXCEPTIONS: (PER FURNISHED TITLE COMMITMENT)
 ITEM #4: THE SUBJECT PROPERTY IS SUBJECT TO A PERPETUAL TRANSPORTATION EASEMENT IN FAVOR OF ALACHUA COUNTY RECORDED SEPTEMBER 10, 1990 IN OFFICIAL RECORDS BOOK 1784, PAGE 2367. (SHOWN GRAPHICALLY HEREON)
 ITEM #5: THE SUBJECT PROPERTY IS SUBJECT TO AN INDEMNIFICATION AGREEMENT IN FAVOR OF ALACHUA COUNTY RECORDED SEPTEMBER 10, 1990 IN OFFICIAL RECORDS BOOK 1784, PAGE 2372.
 ITEM #6: THE SUBJECT PROPERTY IS SUBJECT TO AN EASEMENT IN FAVOR OF CITY OF GAINESVILLE, FLORIDA D/B/A/ GAINESVILLE REGIONAL UTILITIES RECORDED JANUARY 14, 2015 IN OFFICIAL RECORDS BOOK 4324, PAGE 1745. (SHOWN GRAPHICALLY HEREON)

FLOOD ZONE:
 THIS PROPERTY IS LOCATED IN FEDERAL FLOOD ZONE "X". AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS INTERPOLATED FROM F.L.R.M. PANEL NO. 12001CC2840, EFFECTIVE DATE, JUNE 16, 2006



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 est. 1985 FLORIDA LB-5075

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BOOS DEVELOPMENT GROUP, INC.
 CHICAGO TITLE INSURANCE COMPANY
 BRYAN J. STANLEY, P.A.

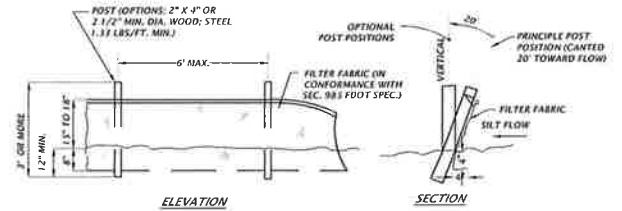
REVISION: 12/18/2015; REVISED TO REFLECT TITLE COMMITMENT INFORMATION (CS)

DATE	11/17/2015
BY	CHW/CHT
REVISION DATE	12/18/2015
BY	MCH/BOO & RAGE
PROJECT NUMBER	481708-25
SHEET NO.	15-0490

This map prepared by: **MICHAEL L. HARBERT**
 Certificate of Authorization No. LB 5075
 NOT VALID WITHOUT THE SIGNATURE AND
 OFFICIAL SEAL OF THE LICENSED SURVEYOR AND MAPPER
 Professional Surveyor & Mapper Fla. License No. 4995

SHEET NO. **1 OF 1**

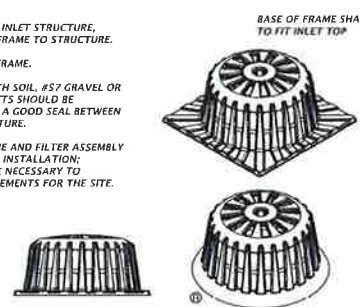
NW 43RD STREET



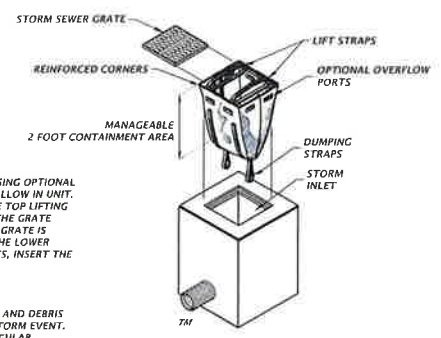
TYPE III SILT FENCE DETAIL

TYPICAL INSTALLATION SEQUENCE FOR SILT-SAVER FRAME AND FILTER

1. EXCAVATE APPROXIMATELY 4" TO 6" BELOW THE TOP OF THE INLET STRUCTURE.
2. PLACE THE FRAME ONTO THE INLET STRUCTURE, ENSURING PROPER SEATING OF FRAME TO STRUCTURE.
3. SLIDE THE FILTER OVER THE FRAME.
4. FILL THE FILTER POCKETS WITH SOIL, #57 GRAVEL OR EQUIVALENT. THE FILTER POCKETS SHOULD BE COMPLETELY FILLED TO ENSURE A GOOD SEAL BETWEEN THE GROUND AND INLET STRUCTURE.
5. BACK FILL AROUND THE FRAME AND FILTER ASSEMBLY IS NOT REQUIRED TO COMPLETE INSTALLATION; HOWEVER, BACK FILLING MAY BE NECESSARY TO COMPLETE EXCAVATION REQUIREMENTS FOR THE SITE.



SILT-SAVER DETAIL



INSTALLATION:
REMOVE THE GRATE FROM INLET. IF USING OPTIONAL OIL ABSORBENTS, PLACE ABSORBENT PILLOW IN UNIT. STAND THE GRATE ON END. MOVE THE TOP LIFTING STRAPS OUT OF THE WAY AND PLACE THE GRATE INTO THE DANDY SACK "SO THAT THE GRATE IS BELOW THE TOP STRAPS AND ABOVE THE LOWER STRAPS. HOLDING THE LIFTING DEVICES, INSERT THE GRATE INTO THE INLET.

MAINTENANCE:
REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM VICINITY OF UNIT AFTER EACH STORM EVENT. AFTER EACH STORM EVENT AND AT REGULAR INTERVALS, LOOK INTO THE DANDY SACK. IF THE CONTAINMENT AREA IS MORE THAN 1/3 FULL OF SEDIMENT, THE UNIT MUST BE EMPTIED. TO EMPTY UNIT, LIFT THE UNIT OUT OF THE INLET USING THE LIFTING STRAPS AND REMOVE THE GRATE. IF USING OPTIONAL OIL ABSORBENTS, REPLACE ABSORBENT WHEN NEAR SATURATION.

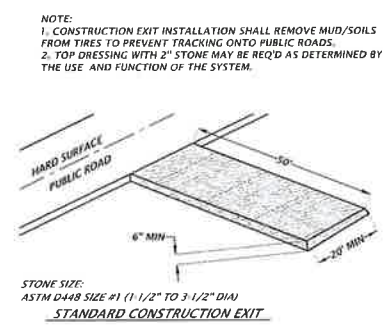
DANDY SACK DETAIL

STORMWATER POLLUTION PREVENTION LEGEND

SD	= SOD STABILIZATION	IP	= INLET PROTECTION
SF	= SILT BARRIER	CO	= CONSTRUCTION ENTRANCE/EXIT

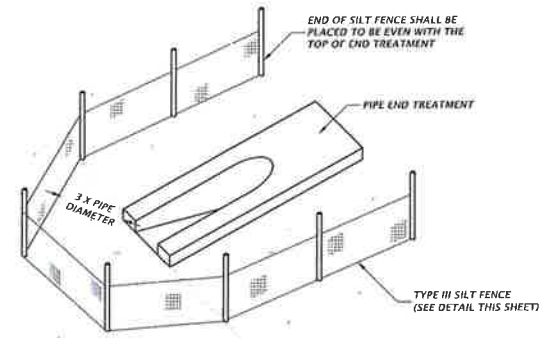
LEGEND:

	PROPOSED STORM SEWER NETWORK
	EXISTING STORM SEWER NETWORK
	LIMITS OF PROPOSED SILT FENCE. SEE THIS SHEET FOR DETAIL.

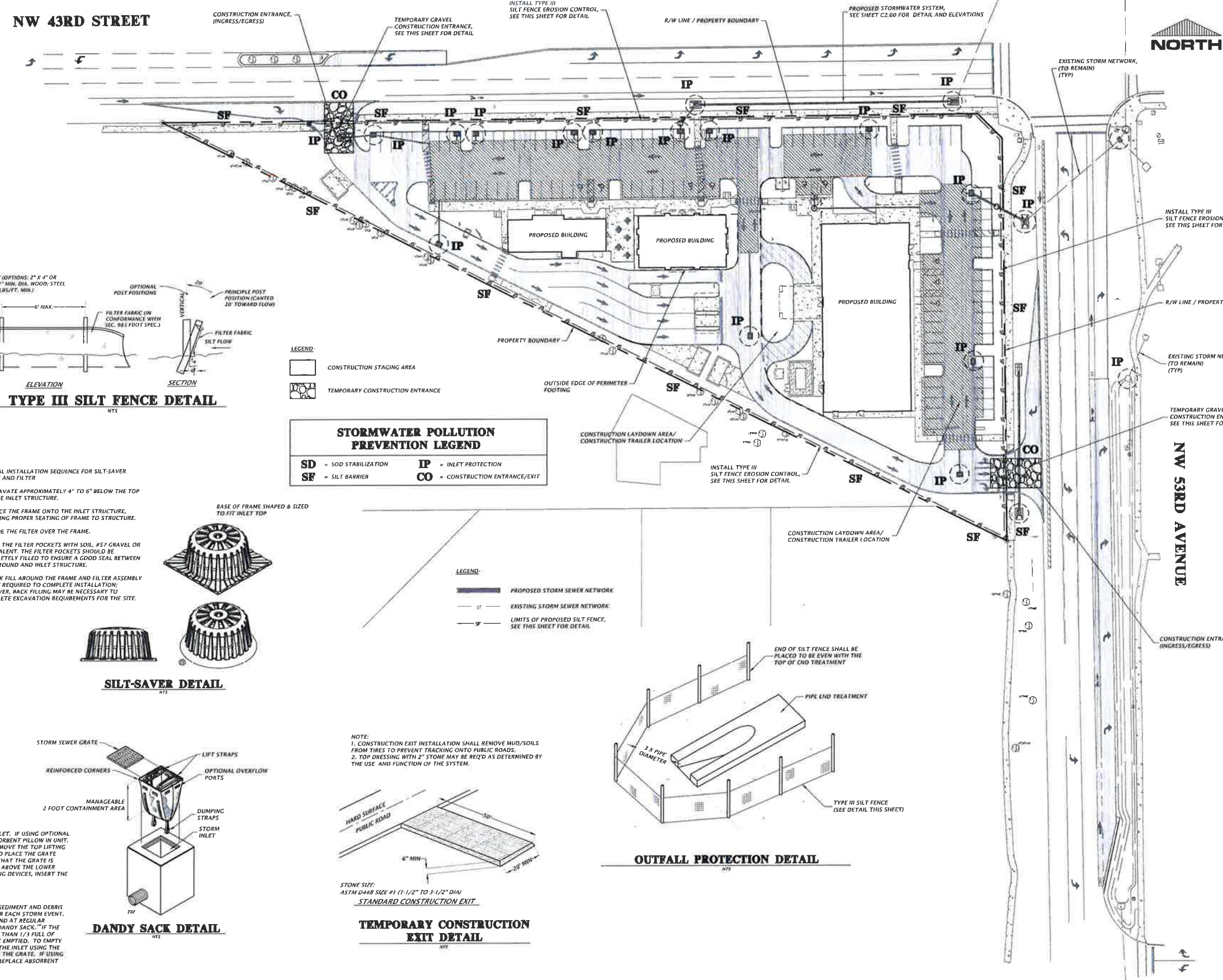


TEMPORARY CONSTRUCTION EXIT DETAIL

NOTE:
1. CONSTRUCTION EXIT INSTALLATION SHALL REMOVE MUD/SOILS FROM TIRES TO PREVENT TRACKING ONTO PUBLIC ROADS.
2. TOP DRESSING WITH 2" STONE MAY BE REQ'D AS DETERMINED BY THE USE AND FUNCTION OF THE SYSTEM.



OUTFALL PROTECTION DETAIL



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Professional Consultants

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est. 1984 FLORIDA CA-5075

SCALE: 1" = 40'

VERTICAL SCALE: 1" = 4'

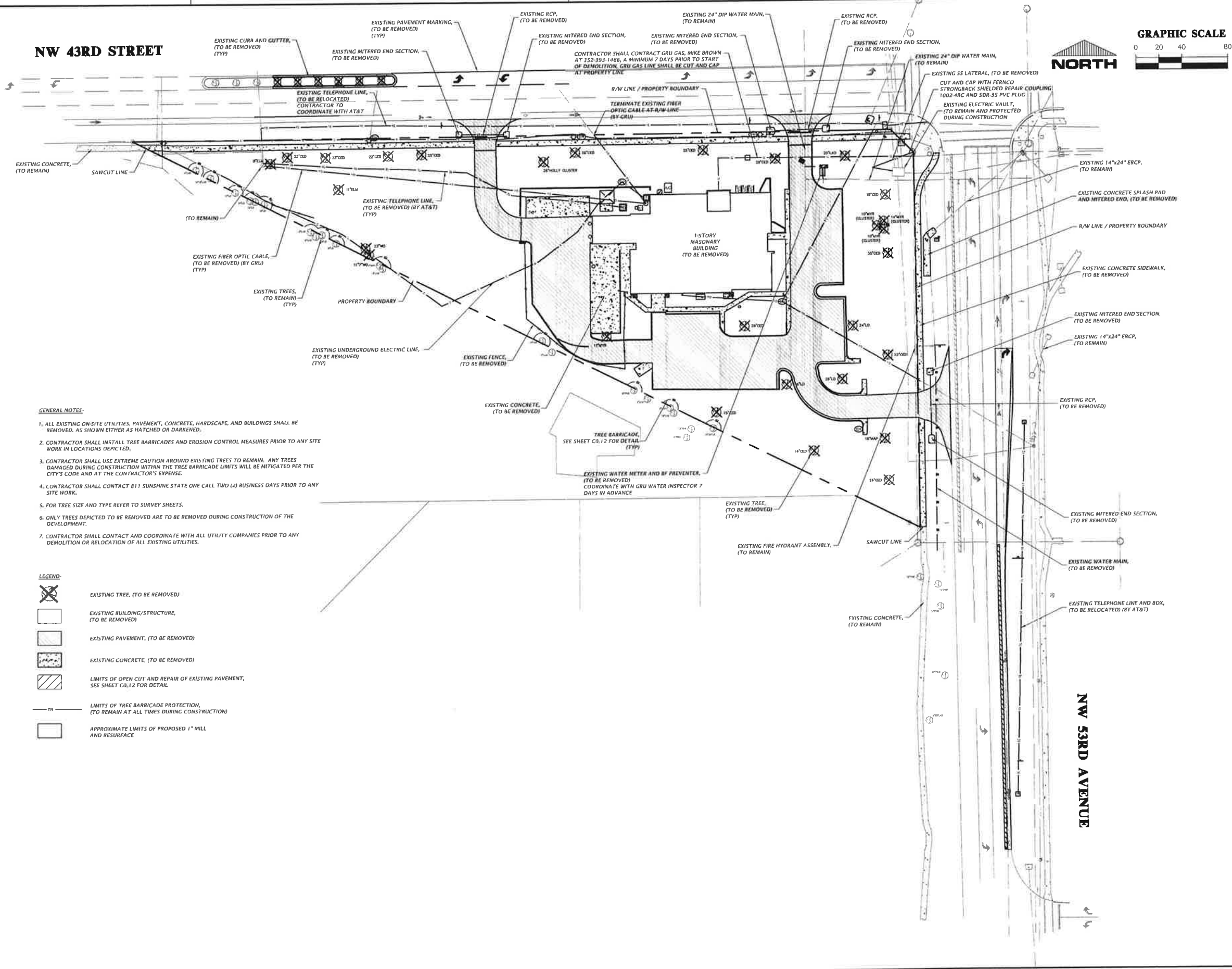
BASE IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON ORIGINAL DRAWING, SCALE ACCORDINGLY.

CLIENT: BOOS DEVELOPMENT GROUP
PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
SHEET TITLE: STORMWATER POLLUTION PREVENTION PLAN
PROJECT NUMBER: 15-0490

DESIGNER: J. THOMAS
CHECKER: R. WALPOLE
IN CHARGE: H. SIVON
PROJECT MANAGER: ROBERT J. WALPOLE

FL PE No. 58206
SHEET NO: C0.21

NW 43RD STREET



GENERAL NOTES:

1. ALL EXISTING ON-SITE UTILITIES, PAVEMENT, CONCRETE, HARDSCAPE, AND BUILDINGS SHALL BE REMOVED, AS SHOWN EITHER AS HATCHED OR DARKENED.
2. CONTRACTOR SHALL INSTALL TREE BARRICADES AND EROSION CONTROL MEASURES PRIOR TO ANY SITE WORK IN LOCATIONS DEPICTED.
3. CONTRACTOR SHALL USE EXTREME CAUTION AROUND EXISTING TREES TO REMAIN. ANY TREES DAMAGED DURING CONSTRUCTION WITHIN THE TREE BARRICADE LIMITS WILL BE MITIGATED PER THE CITY'S CODE AND AT THE CONTRACTOR'S EXPENSE.
4. CONTRACTOR SHALL CONTACT 811 SUNSHINE STATE ONE CALL TWO (2) BUSINESS DAYS PRIOR TO ANY SITE WORK.
5. FOR TREE SIZE AND TYPE REFER TO SURVEY SHEETS.
6. ONLY TREES DEPICTED TO BE REMOVED ARE TO BE REMOVED DURING CONSTRUCTION OF THE DEVELOPMENT.
7. CONTRACTOR SHALL CONTACT AND COORDINATE WITH ALL UTILITY COMPANIES PRIOR TO ANY DEMOLITION OR RELOCATION OF ALL EXISTING UTILITIES.

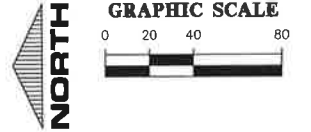
LEGEND:

- EXISTING TREE, (TO BE REMOVED)
- EXISTING BUILDING/STRUCTURE, (TO BE REMOVED)
- EXISTING PAVEMENT, (TO BE REMOVED)
- EXISTING CONCRETE, (TO BE REMOVED)
- LIMITS OF OPEN CUT AND REPAIR OF EXISTING PAVEMENT, SEE SHEET CO.12 FOR DETAIL.
- LIMITS OF TREE BARRICADE PROTECTION, (TO REMAIN AT ALL TIMES DURING CONSTRUCTION)
- APPROXIMATE LIMITS OF PROPOSED 1" MILL AND RESURFACE

PROFESSIONAL CONSULTANTS 152 NW 76th Drive Gainesville, Florida 32607 (352) 381-1976 / (352) 381-2478 www.chm-inc.com est. 1988 FLORIDA CA-5073	
SCALE: 1"=40' VERIFY SCALE BY ORIGINAL DRAWING THIS SHEET, MAINTAIN SCALES ACCORDINGLY.	CONSTRUCTION PERMITS 04/05/16 - 003 AND GRU 05/17/16 - COG, GRU, ALACHUA CO. SURVMO
CLIENT: BOOS DEVELOPMENT GROUP PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT SHEET NO.: 15-0490	DESIGNER: J. THOMAS CHECKER: R. WALPOLE QUALITY CONTROL: H. SIKON PROJECT MANAGER: ROBERT J. WALPOLE
DEMOLITION PLAN	
FL PE No. 68206 SHEET NO. CO.30	

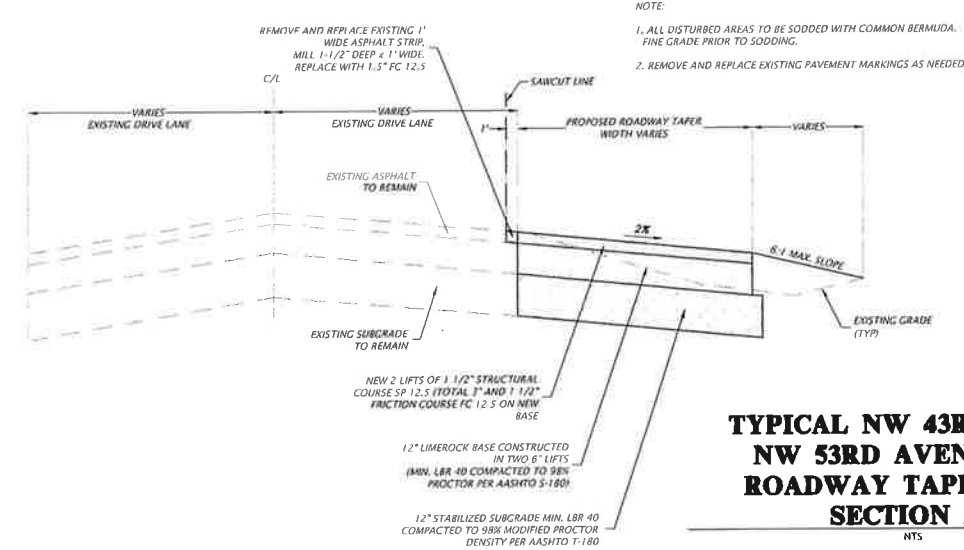
Date: 04/05/16 Plot Date: 05/17/16 File: 15-0490-01.dwg Path: I:\Projects\15-0490\Engineering\15-0490-01.dwg

NW 43RD STREET



- GENERAL NOTES:**
1. SITE LIGHTING TO BE PROVIDED BY POLE MOUNTED LIGHTING FIXTURES. SEE SHEET E-1 FOR DESIGN AND SPECIFICATIONS.
 2. SEE GEOTECHNICAL ENGINEERING REPORT (BY GSE ENGINEERING AND CONSULTING, INC.) FOR GENERAL REQUIREMENTS AND RECOMMENDATIONS.
 3. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR DESIGN AND SPECIFICATIONS FOR HARDSCAPE AREAS.
 4. REFER TO C1.20 FOR HANDICAP ACCESSIBILITY PLAN.
 5. REFER TO LS-1 FOR LANDSCAPE PLAN.
 6. REFER TO ARCHITECTURAL PLANS FOR BUILDING LAYOUT AND ELEVATIONS.

- LEGEND:**
- LIMITS OF PROPOSED CONCRETE. SEE SHEET C0.12 FOR DETAIL.
 - LIMITS OF ASPHALT PAVEMENT. SEE SHEET C0.12 FOR DETAIL.
 - PROPERTY BOUNDARY LINE
 - LIMITS OF PROPOSED BUILDING
 - BUILDING SETBACK / BUILD TO LINE
 - AutoTURN SIMULATION
 - LIMITS OF NW 43RD STREET, NW 53RD AVENUE, AND ROADWAY TAPER ASPHALT PAVEMENT (SEE THIS SHEET FOR DETAIL)



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 Gainesville, FL 32609
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 Professional Consultants
 FLORIDA
 0111888
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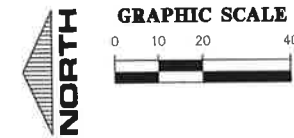
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 VERT. SCALE: 1"=10'
 HORIZ. SCALE: 1"=40'
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DATE: 04/05/16
 05/17/16
 05/17/16

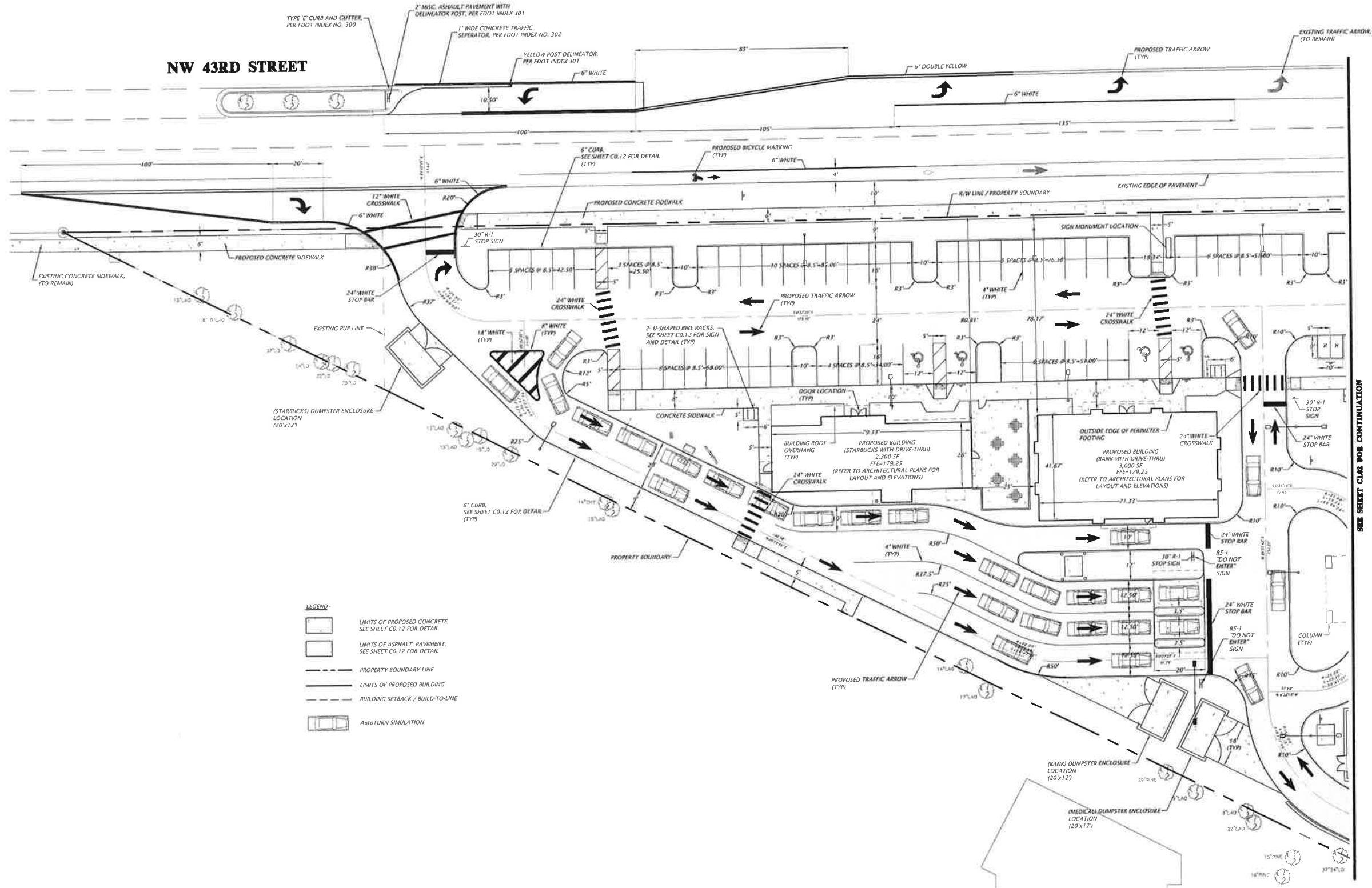
DESIGNER: BOOS DEVELOPMENT GROUP
 PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
 SHEET TITLE: DEVELOPMENT PLAN AND SITE HORIZONTAL CONTROL AND SITE PLAN

EXAMINER: J. THOMAS
 R. WALPOLE
 H. SHON
 PROJECT NO: 15-0490

FL PE No. 58206
 SHEET NO. C1.00



NW 43RD STREET



- LEGEND**
- LIMITS OF PROPOSED CONCRETE, SEE SHEET C0.12 FOR DETAIL
 - LIMITS OF ASPHALT PAVEMENT, SEE SHEET C0.12 FOR DETAIL
 - PROPERTY BOUNDARY LINE
 - LIMITS OF PROPOSED BUILDING
 - BUILDING SETBACK / BUILD-TO-LINE
 - AutoTURN SIMULATION

SEE SHEET C1.0 FOR CONTINUATION

132 NW 76th Drive Gainesville, Florida 32607 (352) 351-1976 / (352) 351-2476 www.ch2m-hill.com CH2M HILL Florida est. 1988	
SCALE: 1"=20' VERIFY SCALE ON ALL DIMENSIONS GENERAL DRAWING 0: DIMENSIONS SHOWN THIS SHEET, ADAPT SCALES ACCORDINGLY.	CONTRACTOR: BOOS DEVELOPMENT GROUP CLIENT: BOOS DEVELOPMENT GROUP PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT SHEET: RETAINED HORIZONTAL CONTROL AND SITE PLAN PROJECT NO.: 15-0490 DATE: 04/05/16 REVISION: 05/17/16 - CCG, GRO, ALACHUA CO. SW/MD
FL PE No. 58206 SHEET NO. C1.01	

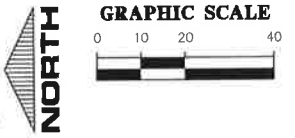
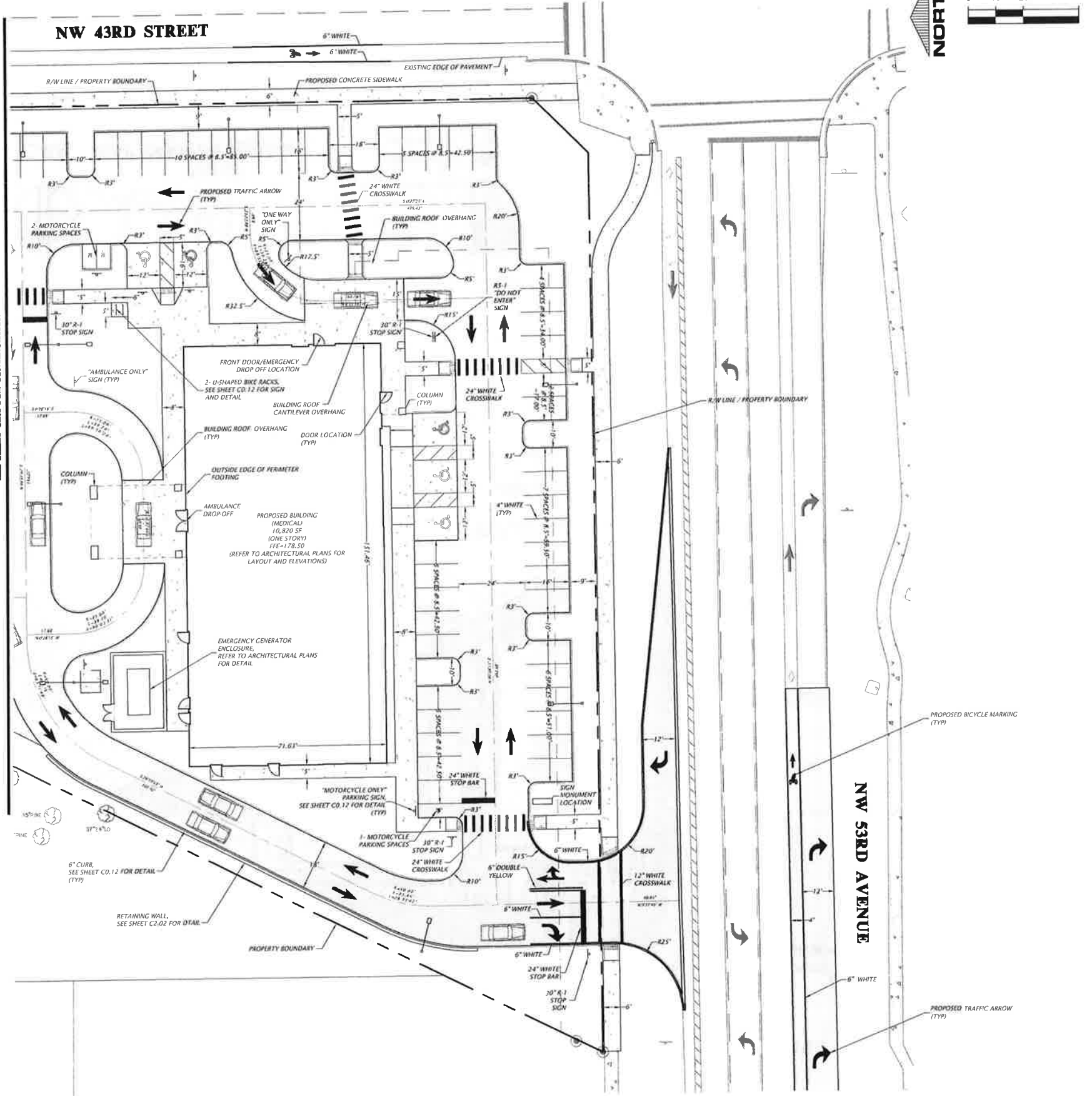
Project Name: Proj Date: 04/13/2016 11:20:18 AM
 User: J:\Users\jwalpole\Documents\Projects\15-0490\C1.01.dwg
 Plot Date: 04/13/2016 11:20:18 AM

DATE: 10/15/18
 FILE: 180718-0490-01.dwg
 PROJECT: NW 43RD STREET AND NW 53RD AVENUE
 DRAWN BY: J. THOMAS
 CHECKED BY: R. WALPOLE
 SCALE: 1"=20'
 SHEET NO.: 15-0490

LEGEND

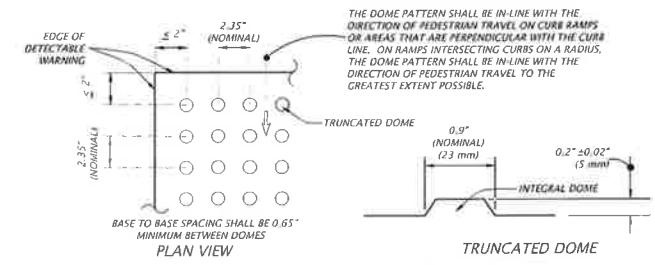
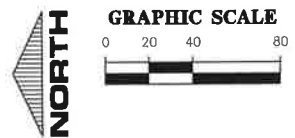
	LIMITS OF PROPOSED CONCRETE, SEE SHEET C0.12 FOR DETAIL
	LIMITS OF ASPHALT PAVEMENT, SEE SHEET C0.12 FOR DETAIL
	PROPERTY BOUNDARY LINE
	LIMITS OF PROPOSED BUILDING
	BUILDING SETBACK / BUILD-TO-LINE
	AutoTURN SIMULATION

SEE SHEET C1.01 FOR CONTINUATION



CHIA	132 NW 76th Drive Gainesville, Florida 32607 (352) 331-1576 / (352) 331-2476 www.chiainc.com est. 1988 FLORIDA CA-3075
SCALE: 1"=20' GRAPHIC SCALE ON THIS SHEET, ADJUST SCALES ACCORDINGLY.	CONSTRUCTION RECORDS CLIENT: BOOS DEVELOPMENT GROUP PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT SHEET: DETAILED HORIZONTAL CONTROL AND SITE PLAN PROJECT NUMBER: 15-0490
REGISTERED PROFESSIONAL ENGINEER J. THOMAS ENGINEER R. WALPOLE PROFESSIONAL CONSULTANT	FL PE No. 58206 SHEET NO. C1.02

NW 43RD STREET

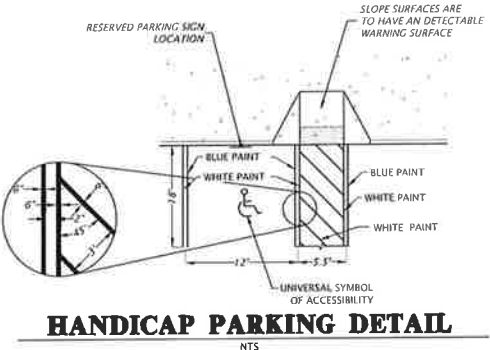


DETECTABLE WARNING DETAIL

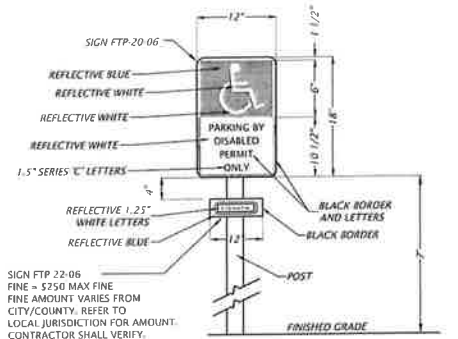
1. PROVIDE DETECTABLE WARNINGS IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) SECTION 4.29 DETECTABLE WARNINGS ON WALKING SURFACES AND THE FLORIDA BUILDING CODE, CHAPTER 11 - FLORIDA ACCESSIBILITY CODE FOR BUILDING CONSTRUCTION PART A, SECTION 11-4.29.
2. RAISED TRUNCATED DOMES SHALL HAVE A DIAMETER OF 0.9 INCH (23 mm) NOMINAL, A HEIGHT OF 0.2 INCH (5 mm) NOMINAL, AND CENTER-TO-CENTER SPACING OF 2.35 INCHES (60 mm) NOMINAL. THE TOP WIDTH OF THE DOME SHALL BE 50% MIN. TO 65% MAX. OF THE BASE DIAMETER.
3. DETECTABLE WARNING COLOR TO PROVIDE LIGHT/DARK CONTRAST BY AT LEAST 70% OF ADJACENT MATERIAL.
4. ALL SIDEWALK CURB RAMPS SHALL HAVE DETECTABLE WARNING SURFACES THAT EXTEND THE FULL WIDTH OF THE RAMP AND IN THE DIRECTION OF TRAVEL 36 INCHES (915 mm).
5. IF MATS ARE TO BE UTILIZED:
 - A - DETECTABLE WARNING SURFACE SHALL CONSIST OF 3 FT WIDE SURFACE APPLIED POLYURETHANE MAT WITH IN-LINE TRUNCATED DOME PATTERN.
 - B - DETECTABLE WARNING SURFACE SHALL BE SECURED BY ADHESIVE AND STAINLESS STEEL ANCHORS.
 - C - ACCEPTABLE PRODUCTS ARE THE SURFACE APPLIED REDIMAT MANUFACTURED BY DETECTABLE WARNING SYSTEMS, INC. OR EQUAL LISTED ON THE FOOT QUALIFIED PRODUCTS LIST IN ACCORDANCE WITH SECTION 5.2.7.2.4 OF THE FDOT STANDARD SPECIFICATIONS.
 - D - PRIOR TO INSTALLING DETECTABLE WARNING MAT, SCRUB THE SURFACE WITH A WIRE BRISTLE BRUSH. THE EXISTING CONCRETE SURFACE SHALL BE CLEANED OF ANY LOOSE MATERIAL, DUST, OILS, GREASE, AND SEALERS.
 - E - ALL INSTALLATIONS SHALL BE MADE IN ACCORDANCE WITH THE DETECTABLE WARNING MAT MANUFACTURER'S SPECIFICATIONS.
6. IF INTERLOCKING PAVERS ARE TO BE UTILIZED:
 - A - DETECTABLE WARNING SURFACE FOR THE RAMPS SHALL CONSIST OF INTERLOCKING 4" X 8" ADA DETECTABLE WARNING SURFACE PAVERS HAVING A MINIMUM DEPTH OF 2" CONCRETE PAVERS ARE TO MEET ASTM C902 CLASS SX TYPE 1; AND BRICK PAVERS ARE TO MEET ASTM C55, GRADE N, SOLID BRICK COLOR TO MEET ADA CONTRAST REQUIREMENTS.
 - B - ALL UNITS SHALL BE SOUND AND FREE OF DEFECTS THAT WOULD INTERFERE WITH THE APPEARANCE OF PROPER PLACEMENT OF THE UNIT OR IMPAIR THE STRENGTH OR LONGEVITY OF THE FINAL STRUCTURE. ANY UNITS THAT ARE STRUCTURALLY DAMAGED DURING THE WORK SHALL BE IMMEDIATELY REMOVED AND REPLACED. THE PAVERS ARE TO BE LAID IN A TWO BY TWO BASKETWEAVE PATTERN, FLUSH WITH THE FINISH GRADE OF THE RAMP SURFACE. AND HAVE GAPS BETWEEN 1/16 AND 1/8 INCH. CUT PAVERS (MASONRY SAW ONLY) SHALL BE NO SMALLER THAN ONE THIRD OF A WHOLE PAVER.
 - C - MODIFY FORMWORK OR PROVIDE FORMED GROUT INFILL BEHIND CURVED DROP CURB SECTIONS TO ENSURE STRAIGHT EDGE RESTRAINT FOR PAVERS.
 - D - CONCRETE EDGE RESTRAINT FOR WARNING AREA. MAXIMUM GAP OF 1/8" BETWEEN PAVERS AND EDGE. USE 1/4" RADIUS ALONG CONCRETE EDGES.
 - E - WHEN PAVERS ADJUT EITHER EARTH/DIRT OR PAVEMENT, A SIX INCH CONCRETE BAND SHALL BE INSTALLED. BAND SHALL BE WITHIN LIMITS OF THE WIDTH OF PROPOSED CONCRETE AND NOT EXTENDED AN ADDITIONAL SIX INCHES.
7. OTHER METHODS/MATERIALS MAY BE USED FOR THE DETECTABLE WARNING STRIP, BUT THEY MUST MEET THE ABOVE CRITERIA.
8. ALL MATERIALS/METHODS TO BE UTILIZED SHALL BE SUBMITTED FOR APPROVAL TO THE ENGINEER PRIOR TO ORDERING THE MATERIALS/METHODS. FAILURE TO OBTAIN APPROVAL BEFORE ORDERING OR INSTALLATION MAY RESULT IN REMOVAL AND REPLACEMENT AT CONTRACTOR'S EXPENSE. ENGINEER'S APPROVAL DOES NOT RELIEVE THE CONTRACTOR'S RESPONSIBILITY FOR THE PERFORMANCE OF THE ITEM.
9. IF FDOT CURB RAMPS ARE SPECIFIED, REFER TO THE FDOT INDEX NO. 304 FOR SPECIFICATIONS CONCERNING THE APPLICABLE DETECTABLE WARNING STRIP TO BE USED WITH FDOT SPECIFIED RAMPS.

GENERAL NOTE:
 1. ALL HANDICAP ACCESSIBLE PARKING SPACES, HANDICAP ACCESSIBLE SIDEWALK ROUTES AND ALL SITE SIDEWALKS ARE NOT TO EXCEED A 2% MAXIMUM CROSS SLOPE.

LEGEND:
 ■■■■■ ACCESSIBLE HANDICAP PATH

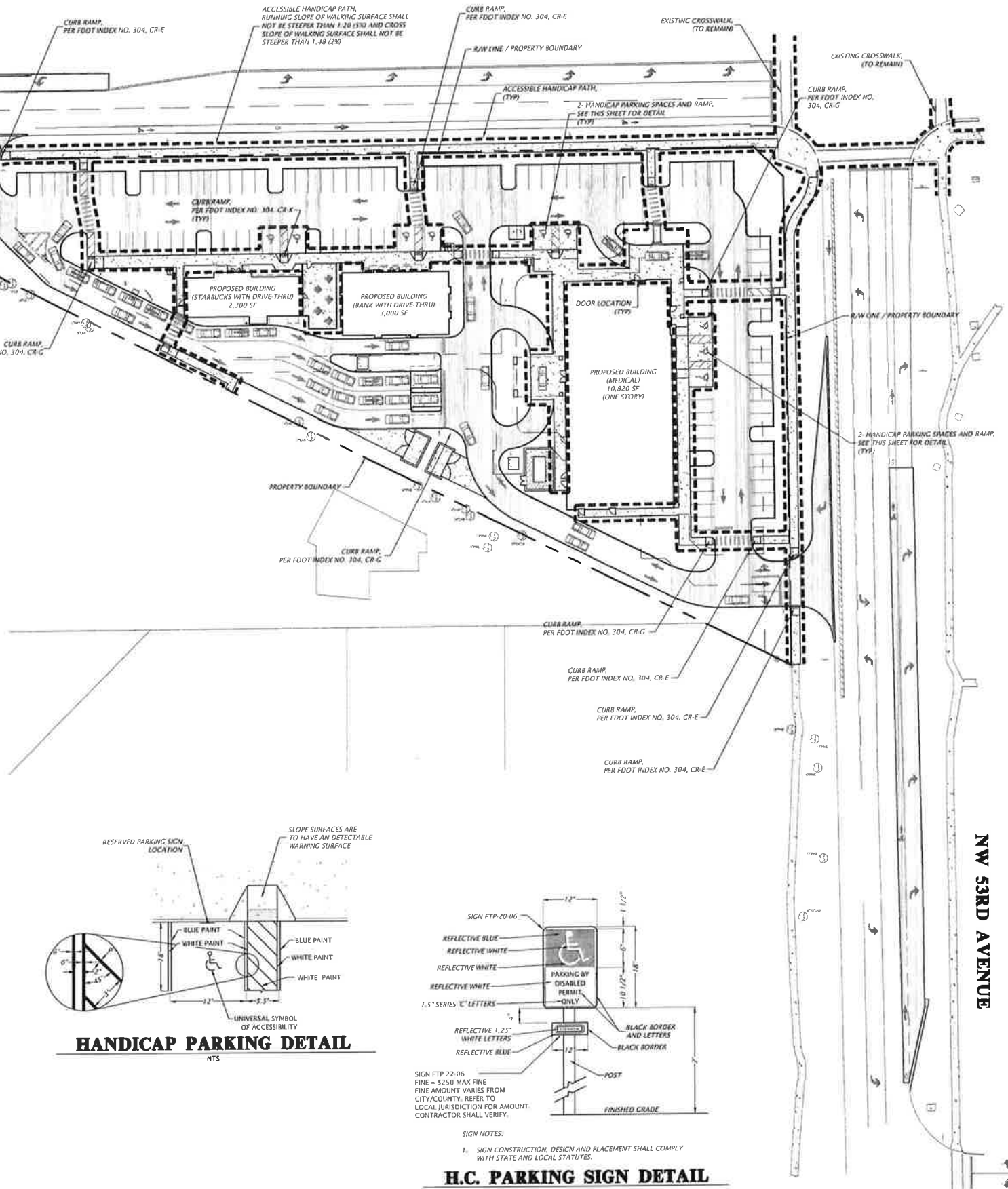


HANDICAP PARKING DETAIL



SIGN NOTES:
 1. SIGN CONSTRUCTION, DESIGN AND PLACEMENT SHALL COMPLY WITH STATE AND LOCAL STATUTES.

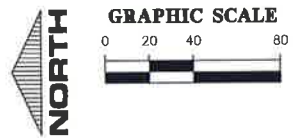
H.C. PARKING SIGN DETAIL



NW 53RD AVENUE

132 NW 76th Drive
 Gainesville, FL 32609
 (352) 381-1978 / (352) 381-2078
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 est. 1988 FLORIDA
 CHW
 Professionals | Consultants
 SCALE: 1"=40'
 VERIFY SCALE OR BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON SCALE, ACCORDINGLY.
 SUBMITTAL: 04/05/16 - 000 AND GRU - COG, GRU, ALACHUA CO. SURVMO
 PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
 SHEET TITLE: ACCESSIBILITY SITE PLAN AND DETAILS
 PROJECT NUMBER: 15-0490
 DESIGNER: J. THOMAS
 CHECKER: R. WALPOLE
 DRAWN BY: ROBERT J. WALPOLE
 PROJECT MANAGER: H. SHOWN
 FL PE No. 58206
 SHEET NO. C1.20

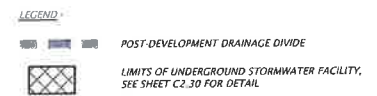
NW 43RD STREET



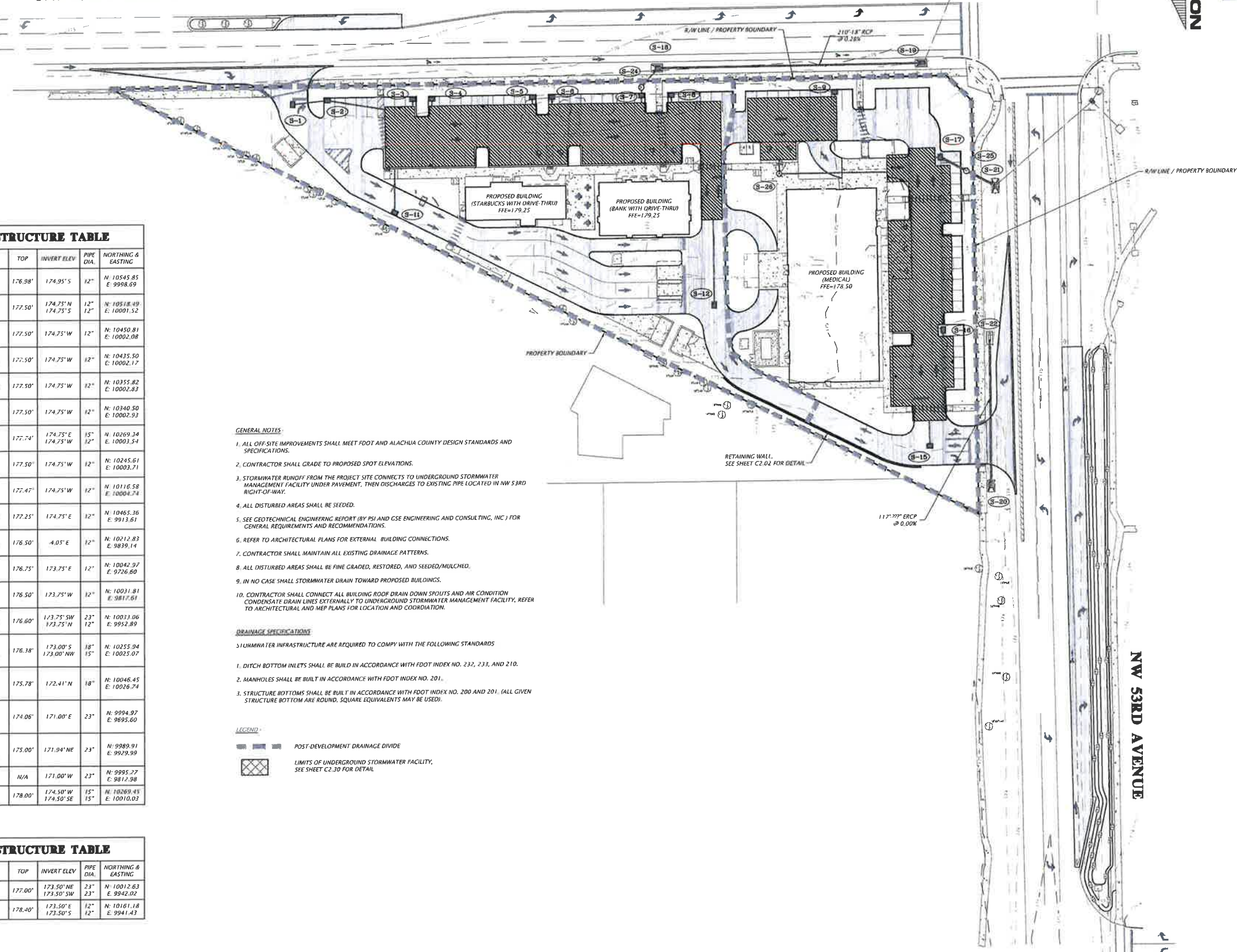
STORMWATER STRUCTURE TABLE					
STRUCTURE	STRUCTURE TYPE	TOP	INVERT ELEV	PIPE DIA.	NORTHING & EASTING
S-1	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	176.98'	174.95'S	12"	N: 10545.85 E: 9998.69
S-2	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.50'	174.75'N 174.75'S	12"	N: 10518.49 E: 10001.52
S-3	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.50'	174.75'W	12"	N: 10450.81 E: 10002.08
S-4	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.50'	174.75'W	12"	N: 10435.50 E: 10002.17
S-5	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.50'	174.75'W	12"	N: 10355.82 E: 10002.83
S-6	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.50'	174.75'W	12"	N: 10340.50 E: 10002.93
S-7	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.74'	174.75'E 174.75'W	15"	N: 10269.34 E: 10003.54
S-8	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.50'	174.75'W	12"	N: 10245.61 E: 10003.71
S-9	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.47'	174.75'W	12"	N: 10116.58 E: 10004.74
S-11	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	177.25'	174.75'E	12"	N: 10465.36 E: 9913.61
S-12	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	176.50'	-4.05'E	12"	N: 10212.83 E: 9839.14
S-15	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	176.75'	173.75'E	12"	N: 10042.97 E: 9726.80
S-16	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	176.50'	173.75'W	12"	N: 10031.81 E: 9817.61
S-17	TYPE "F" INLET WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	176.60'	173.75'SW 173.75'N	23"	N: 10033.06 E: 9952.89
S-18	TYPE "C" INLET WITH SINGLE TRAVERSABLE SLOT WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	176.38'	173.00'S 173.00'NW	15"	N: 10255.94 E: 10025.07
S-19	TYPE "C" INLET WITH SINGLE TRAVERSABLE SLOT WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	175.78'	172.41'N	10"	N: 10046.45 E: 10026.74
S-20	TYPE "C" INLET WITH SINGLE TRAVERSABLE SLOT WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	174.06'	171.00'E	23"	N: 9994.97 E: 9695.60
S-21	TYPE "C" INLET WITH DOUBLE TRAVERSABLE SLOT WITH 1" BOTTOM PER FDOT INDEX NO. 233 & 210	175.00'	171.94'NE	23"	N: 9989.91 E: 9929.99
S-22	18" MITERED END SECTION PER FDOT INDEX NO. 272	N/A	171.00'W	23"	N: 9995.27 E: 9812.98
S-24	MANHOLE PER FDOT INDEX NO. 200 & 201	178.00'	174.50'W 174.50'SE	15"	N: 10269.43 E: 10010.03

- GENERAL NOTES**
- ALL OFF-SITE IMPROVEMENTS SHALL MEET FDOT AND ALACHUA COUNTY DESIGN STANDARDS AND SPECIFICATIONS.
 - CONTRACTOR SHALL GRADE TO PROPOSED SPOT ELEVATIONS.
 - STORMWATER RUNOFF FROM THE PROJECT SITE CONNECTS TO UNDERGROUND STORMWATER MANAGEMENT FACILITY UNDER PAVEMENT, THEN DISCHARGES TO EXISTING PIPE LOCATED IN NW 53RD RIGHT-OF-WAY.
 - ALL DISTURBED AREAS SHALL BE SEEDED.
 - SEE GEOTECHNICAL ENGINEERING REPORT (BY PSI AND GSE ENGINEERING AND CONSULTING, INC.) FOR GENERAL REQUIREMENTS AND RECOMMENDATIONS.
 - REFER TO ARCHITECTURAL PLANS FOR EXTERNAL BUILDING CONNECTIONS.
 - CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE PATTERNS.
 - ALL DISTURBED AREAS SHALL BE FINE GRADED, RESTORED, AND SEEDED/MULCHED.
 - IN NO CASE SHALL STORMWATER DRAIN TOWARD PROPOSED BUILDINGS.
 - CONTRACTOR SHALL CONNECT ALL BUILDING ROOF DRAIN DOWN SPOUTS AND AIR CONDITION CONDENSATE DRAIN LINES EXTERNALLY TO UNDERGROUND STORMWATER MANAGEMENT FACILITY. REFER TO ARCHITECTURAL AND MEP PLANS FOR LOCATION AND COORDINATION.

- DRAINAGE SPECIFICATIONS**
- DITCH BOTTOM INLETS SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 232, 233, AND 210.
 - MANHOLES SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 201.
 - STRUCTURE BOTTOMS SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 200 AND 201. (ALL GIVEN STRUCTURE BOTTOMS ARE ROUND. SQUARE EQUIVALENTS MAY BE USED).



STORMWATER STRUCTURE TABLE					
STRUCTURE	STRUCTURE TYPE	TOP	INVERT ELEV	PIPE DIA.	NORTHING & EASTING
S-25	MANHOLE PER FDOT INDEX NO. 200 & 201	177.00'	173.50'NE 173.50'SW	23"	N: 10012.63 E: 9942.02
S-26	MANHOLE PER FDOT INDEX NO. 200 & 201	178.40'	173.50'E 173.50'S	12"	N: 10161.18 E: 9941.43



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Professional Consultants

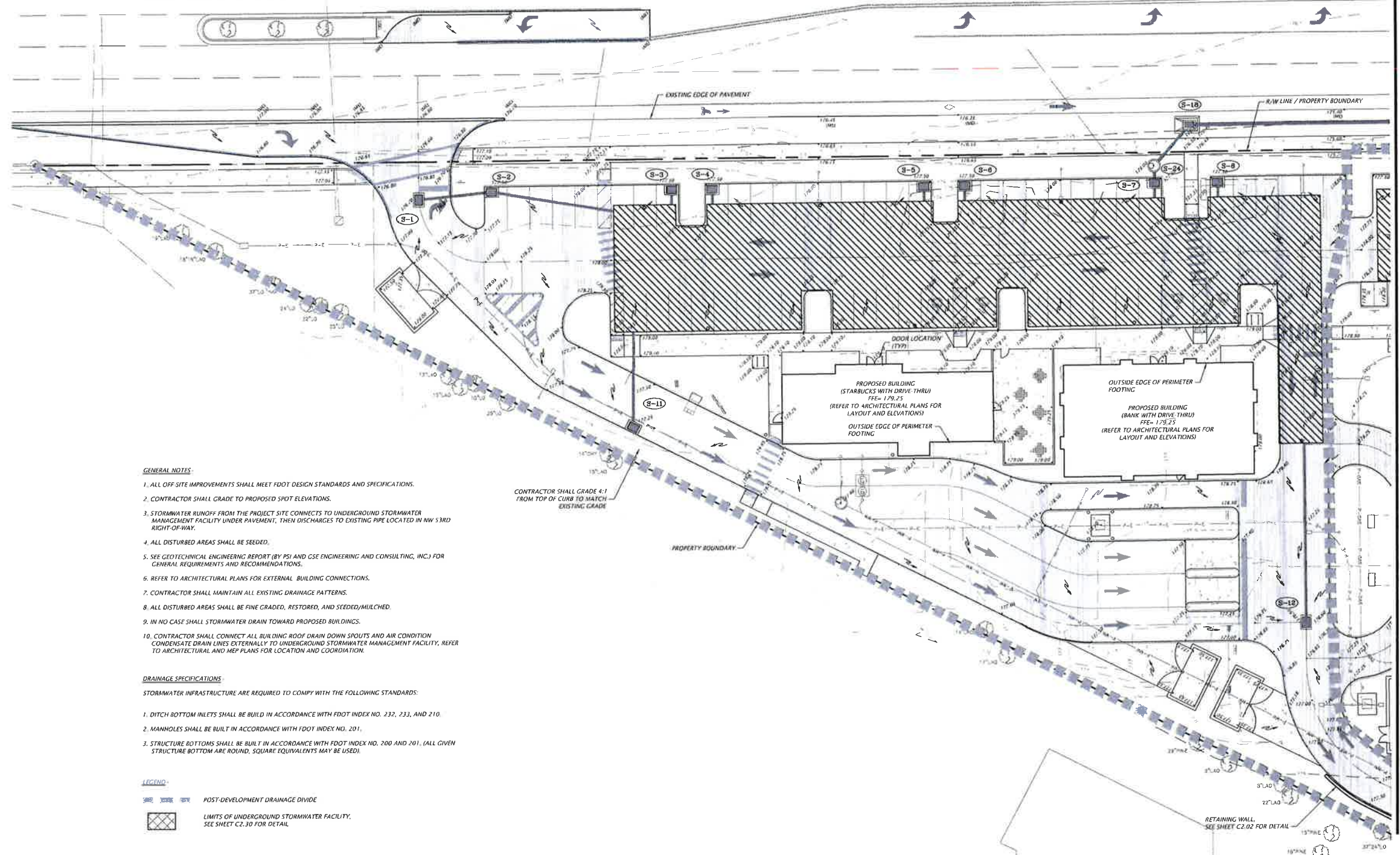
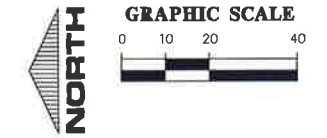
SCALE: 1"=40'
VERTICAL SCALE: 1"=10'
HORIZONTAL SCALE: AS SHOWN
IF NOT ONE HUNDRED PERCENT ACCORDINGLY.

SUBMITTAL: 04/05/18 - CDD AND GRU
05/17/18 - CDD, GRU, ALACHUA CO. SURVIMD

CLIENT: BOOS DEVELOPMENT GROUP	PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
DESIGNER: J. THOMAS	QUALITY CONTROL: H. SIMON
EXCHANGER: R. WALPOLE	PROJECT NUMBER: 13-0490
ROBERT J. WALPOLE	

FL PE No. 58206
SHEET NO. C2.00

NW 43RD STREET



GENERAL NOTES:

1. ALL OFF SITE IMPROVEMENTS SHALL MEET FDOT DESIGN STANDARDS AND SPECIFICATIONS.
2. CONTRACTOR SHALL GRADE TO PROPOSED SPOT ELEVATIONS.
3. STORMWATER RUNOFF FROM THE PROJECT SITE CONNECTS TO UNDERGROUND STORMWATER MANAGEMENT FACILITY UNDER PAVEMENT, THEN DISCHARGES TO EXISTING PIPE LOCATED IN NW 53RD RIGHT-OF-WAY.
4. ALL DISTURBED AREAS SHALL BE SEEDED.
5. SEE GEOTECHNICAL ENGINEERING REPORT (BY PSI AND CSE ENGINEERING AND CONSULTING, INC.) FOR GENERAL REQUIREMENTS AND RECOMMENDATIONS.
6. REFER TO ARCHITECTURAL PLANS FOR EXTERNAL BUILDING CONNECTIONS.
7. CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE PATTERNS.
8. ALL DISTURBED AREAS SHALL BE FINE GRADED, RESTORED, AND SEEDED/MULCHED.
9. IN NO CASE SHALL STORMWATER DRAIN TOWARD PROPOSED BUILDINGS.
10. CONTRACTOR SHALL CONNECT ALL BUILDING ROOF DRAIN DOWN SPOUTS AND AIR CONDITION CONDENSATE DRAIN LINES EXTERNALLY TO UNDERGROUND STORMWATER MANAGEMENT FACILITY. REFER TO ARCHITECTURAL AND MEP PLANS FOR LOCATION AND COORDINATION.

DRAINAGE SPECIFICATIONS:

STORMWATER INFRASTRUCTURE ARE REQUIRED TO COMPLY WITH THE FOLLOWING STANDARDS:

1. DITCH BOTTOM INLETS SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 232, 233, AND 210.
2. MANHOLES SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 201.
3. STRUCTURE BOTTOMS SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 200 AND 201. (ALL GIVEN STRUCTURE BOTTOM ARE ROUND, SQUARE EQUIVALENTS MAY BE USED).

LEGEND:

- POST-DEVELOPMENT DRAINAGE DIVIDE
- LIMITS OF UNDERGROUND STORMWATER FACILITY. SEE SHEET C2.30 FOR DETAIL.

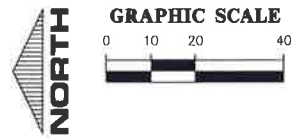
CONTRACTOR SHALL GRADE 4:1 FROM TOP OF CURB TO MATCH EXISTING GRADE

PROPERTY BOUNDARY

RETAINING WALL. SEE SHEET C2.02 FOR DETAIL

SEE SHEET C2.02 FOR CONTINUATION

<p>132 NW 76th Drive Gainesville, FL 32609 (882) 331-1878 / (882) 331-2478 WWW.CH2M.HILL.COM</p>	<p>CH2M HILL Professional Consultants</p>	<p>SCALE: 1" = 20' VERTICAL SCALE: 1" = 10' 0 10 20 30 40 # NOT ONE INCH DIV SCALES ACCORDINGLY.</p>	<p>SUBMITTALS: 04/05/18 - CGG AND GRG 05/17/18 - CGG, GRG, ALACHUA CO. SRMWD</p> <p>CLIENT: BOOS DEVELOPMENT GROUP PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT SHEET TITLE: DETAILED DRAINAGE AND GRADING PLAN</p> <p>DESIGNER: J. THOMAS CHECKER: R. WALPOLE COUNTY CONTROL: FLORIDA DEPARTMENT OF TRANSPORTATION PROJECT NUMBER: 15-0490</p> <p>FL PE No. 58206 SHEET NO.: C2.01</p>
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GENERAL NOTES

1. ALL OFF-SITE IMPROVEMENTS SHALL MEET FDOT DESIGN STANDARDS AND SPECIFICATIONS.
2. CONTRACTOR SHALL GRADE TO PROPOSED SPOT ELEVATIONS.
3. STORMWATER RUNOFF FROM THE PROJECT SITE CONNECTS TO UNDERGROUND STORMWATER MANAGEMENT FACILITY UNDER PAVEMENT. THEN DISCHARGES TO EXISTING PIPE LOCATED IN NW 53RD RIGHT-OF-WAY.
4. ALL DISTURBED AREAS SHALL BE SEEDDED.
5. SEE GEOTECHNICAL ENGINEERING REPORT (BY PSI AND GSE ENGINEERING AND CONSULTING, INC.) FOR GENERAL REQUIREMENTS AND RECOMMENDATIONS.
6. REFER TO ARCHITECTURAL PLANS FOR EXTERNAL BUILDING CONNECTIONS.
7. CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE PATTERNS.
8. ALL DISTURBED AREAS SHALL BE FINE GRADED, RESTORED, AND SEEDDED/MULCHED.
9. IN NO CASE SHALL STORMWATER DRAIN TOWARD PROPOSED BUILDINGS.
10. CONTRACTOR SHALL CONNECT ALL BUILDING ROOF DRAIN DOWN SPOUTS AND AIR CONDITION CONDENSATE DRAIN LINES EXTERNALLY TO UNDERGROUND STORMWATER MANAGEMENT FACILITY. REFER TO ARCHITECTURAL AND MEP PLANS FOR LOCATION AND COORDINATION.

DRAINAGE SPECIFICATIONS

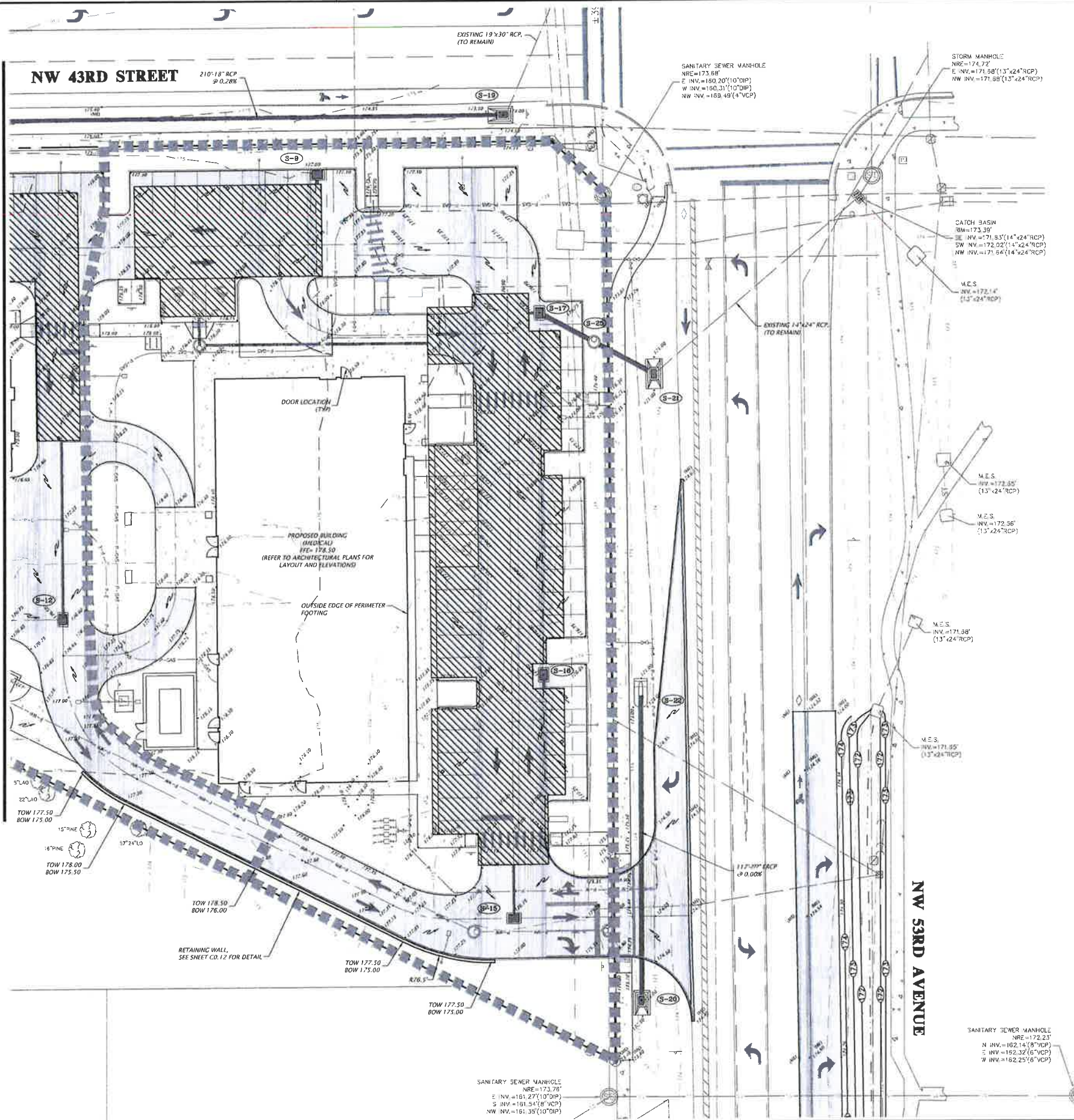
STORMWATER INFRASTRUCTURE ARE REQUIRED TO COMPLY WITH THE FOLLOWING STANDARDS:

1. DITCH BOTTOM INLETS SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 232, 233, AND 210.
2. MANHOLES SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 201.
3. STRUCTURE BOTTOMS SHALL BE BUILT IN ACCORDANCE WITH FDOT INDEX NO. 200 AND 201. (ALL GIVEN STRUCTURE BOTTOM ARE ROUND, SQUARE EQUIVALENTS MAY BE USED).

LEGEND

- POST-DEVELOPMENT DRAINAGE DIVIDE
- LIMITS OF UNDERGROUND STORMWATER FACILITY. SEE SHEET C2.30 FOR DETAIL.

SEE SHEET C2.11 FOR CONTINUATION



500 NW 76th Ave
 Gainesville, Florida 32607
 (850) 351-1976 / (850) 351-2476
 www.chw-inc.com
 est. 1988 FLORIDA
 CH-0079

CHW
 Professional Consultants

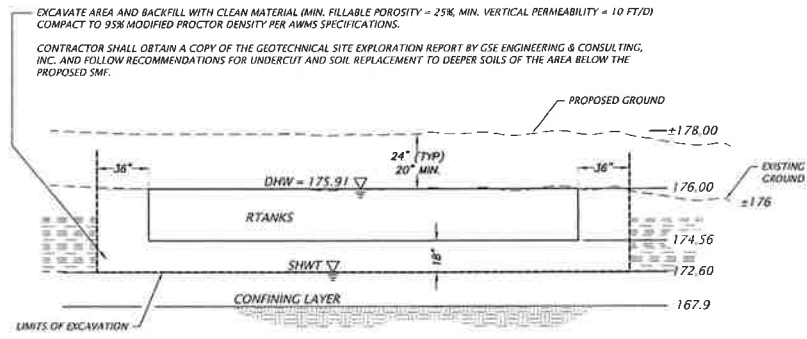
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 IF ANY ONE MATCHES ON
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SUBMITTALS:
 04/05/16 - CGC AND GRU
 02/17/16 - CGC, GRU, ALACHUA CO. SURVINO

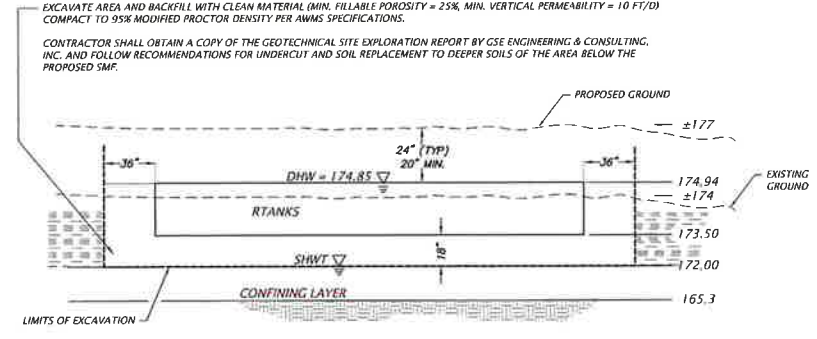
CLIENT: BOOS DEVELOPMENT GROUP
 PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
 SHEET TITLE: DETAILED DRAINAGE AND GRADING PLAN
 PROJECT NUMBER: 15-0490

DESIGNER: J. THOMAS
 CHECKER: R. WALPOLE
 PROJECT MANAGER: H. SNOOK
 ROBERT J. WALPOLE

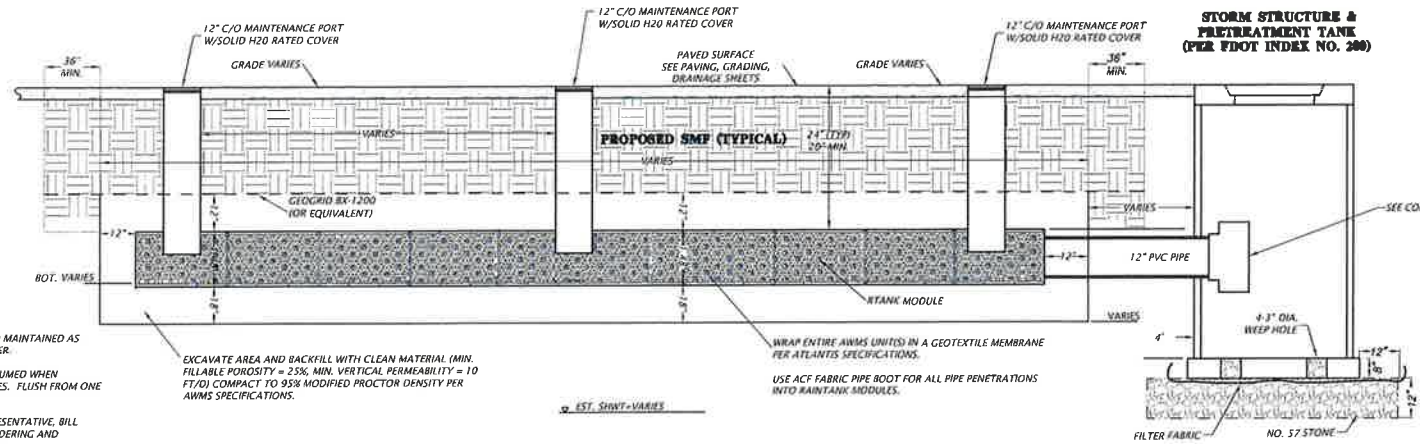
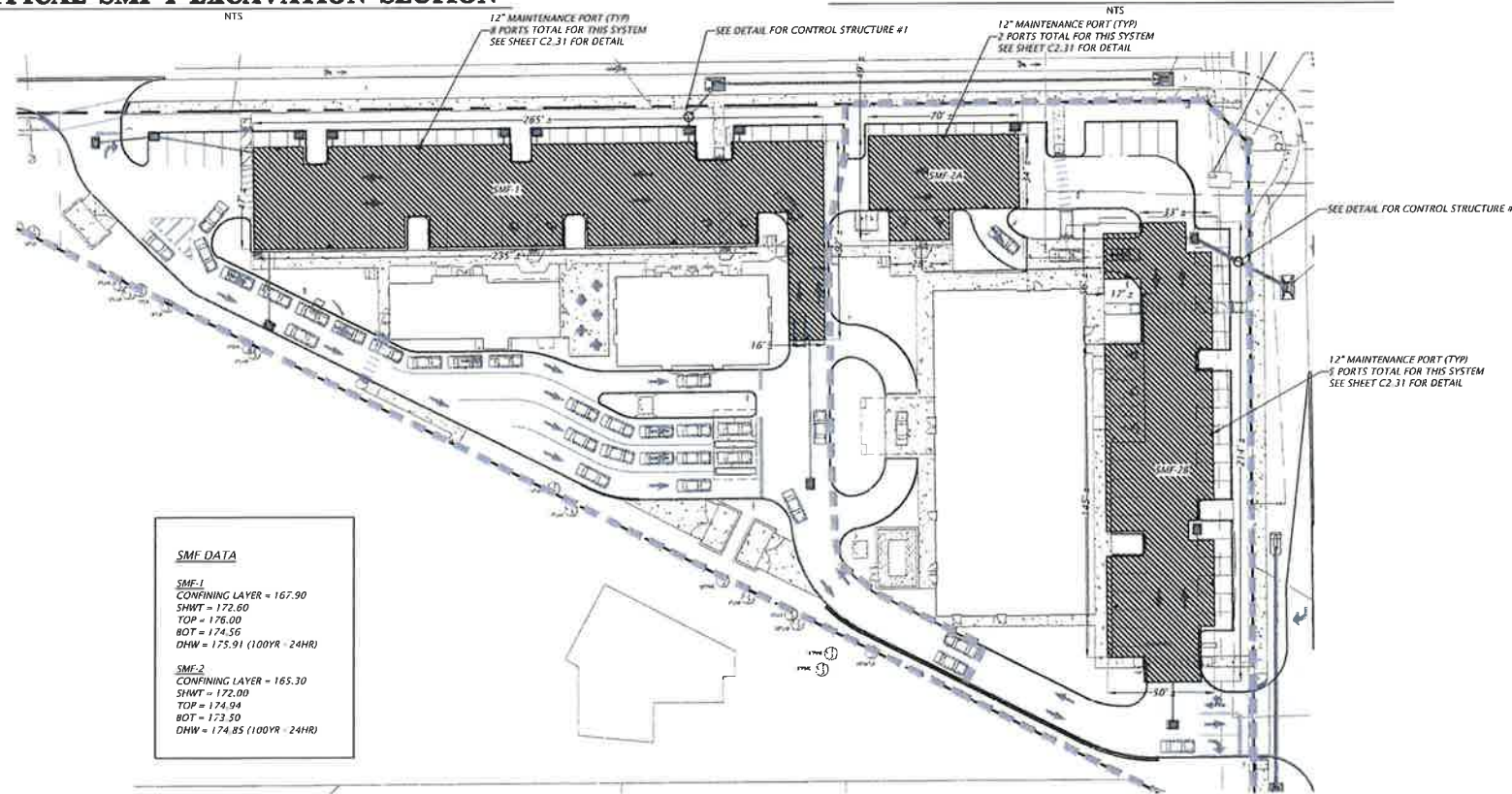
FL PE No. 58206
 SHEET NO.: C2.02



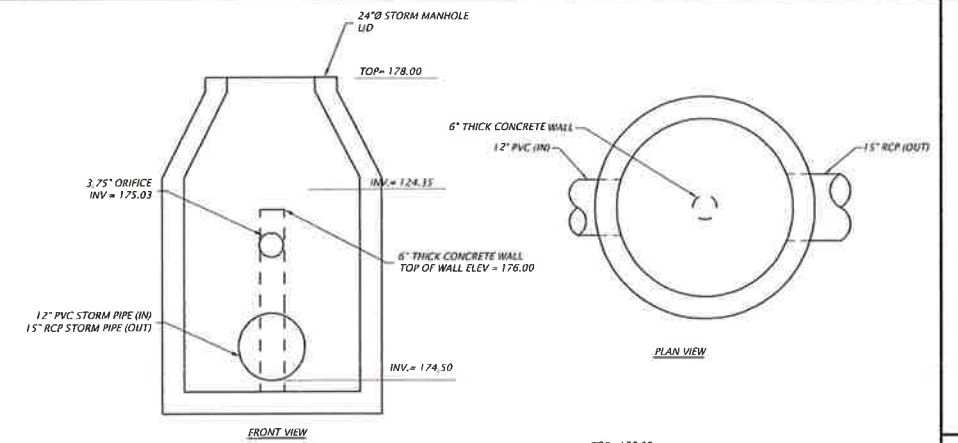
TYPICAL SMF-1 EXCAVATION SECTION



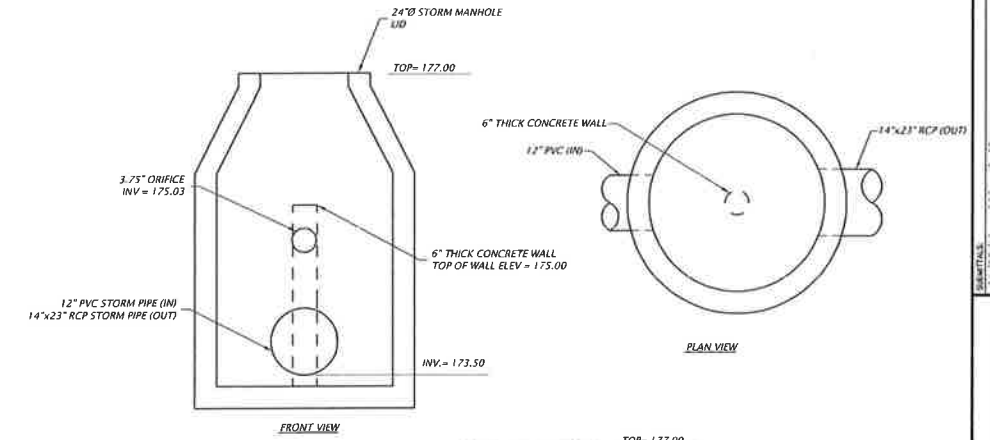
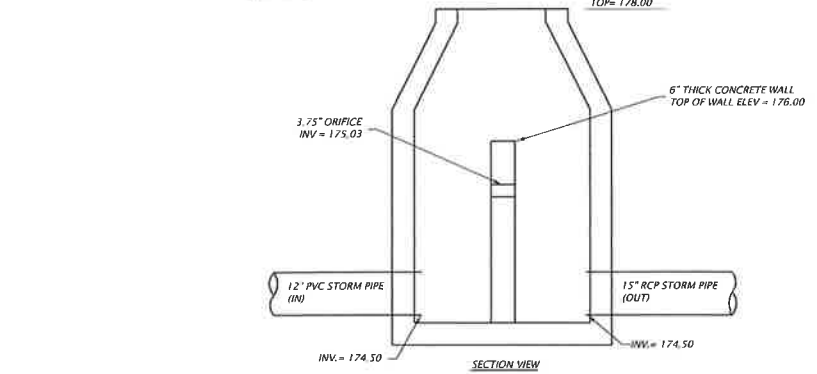
TYPICAL SMF-2 EXCAVATION SECTION



RTANK UNDERGROUND STORMWATER MANAGEMENT FACILITY DETAIL



CONTROL STRUCTURE #1 DETAIL



CONTROL STRUCTURE #2 DETAIL

INSPECTION / MAINTENANCE NOTE:
THE SYSTEM SHALL BE INSPECTED AND MAINTAINED AS RECOMMENDED BY THE MANUFACTURER.
SYSTEM SHALL BE FLUSHED AND VACUUMED WHEN SEDIMENT OR DEBRIS EXCEEDS 2 INCHES. FLUSH FROM ONE END AND VACUUM FROM OTHER.
CONTACT ACF ENVIRONMENTAL REPRESENTATIVE, BILL FARMER, AT 1 (800) 448-3636 FOR ORDERING AND ADDITIONAL PRODUCT INFORMATION

RTANK UNDERGROUND STORMWATER MANAGEMENT FACILITY DETAIL

132 NW 76th Drive
Gainesville, FL 32608
(352) 351-1976
WWW.CHW-FL.COM

CHW
Professional Consultants
1888 FLORIDA
CA-8075

SCALE: N/A
VERIFY SCALE FOR ALL DIMENSIONS ON THIS SHEET. DO NOT SCALE DIMENSIONS FROM THIS SHEET.

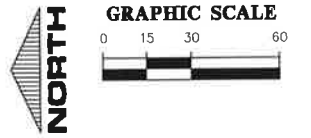
DATE: 04/05/18 - 000 AND GRG
05/17/18 - 000, GRG, ALACHUA CO. SRWMD

CLIENT: N/A
DEVELOPMENT GROUP: N/A
PROJECT: NW 33RD STREET AND NW 33RD AVENUE RETAIL DEVELOPMENT
SHEET TITLE: STORMWATER MANAGEMENT FACILITY PLAN AND DETAILS
PROJECT NUMBER: 15-0490

TECHNICAL: J. THOMAS
DESIGNER: J. THOMAS
CHECKER: H. SMON
PROJECT NUMBER: 15-0490

FL PE No. 58206
SHEET NO.: C2.30

NW 43RD STREET



150 NW 7th Ave
Gainesville, Florida 32607
(352) 331-1976 (352) 331-2476
www.chw-inc.com
est. 1988 FLORIDA
CA-50773

CHW

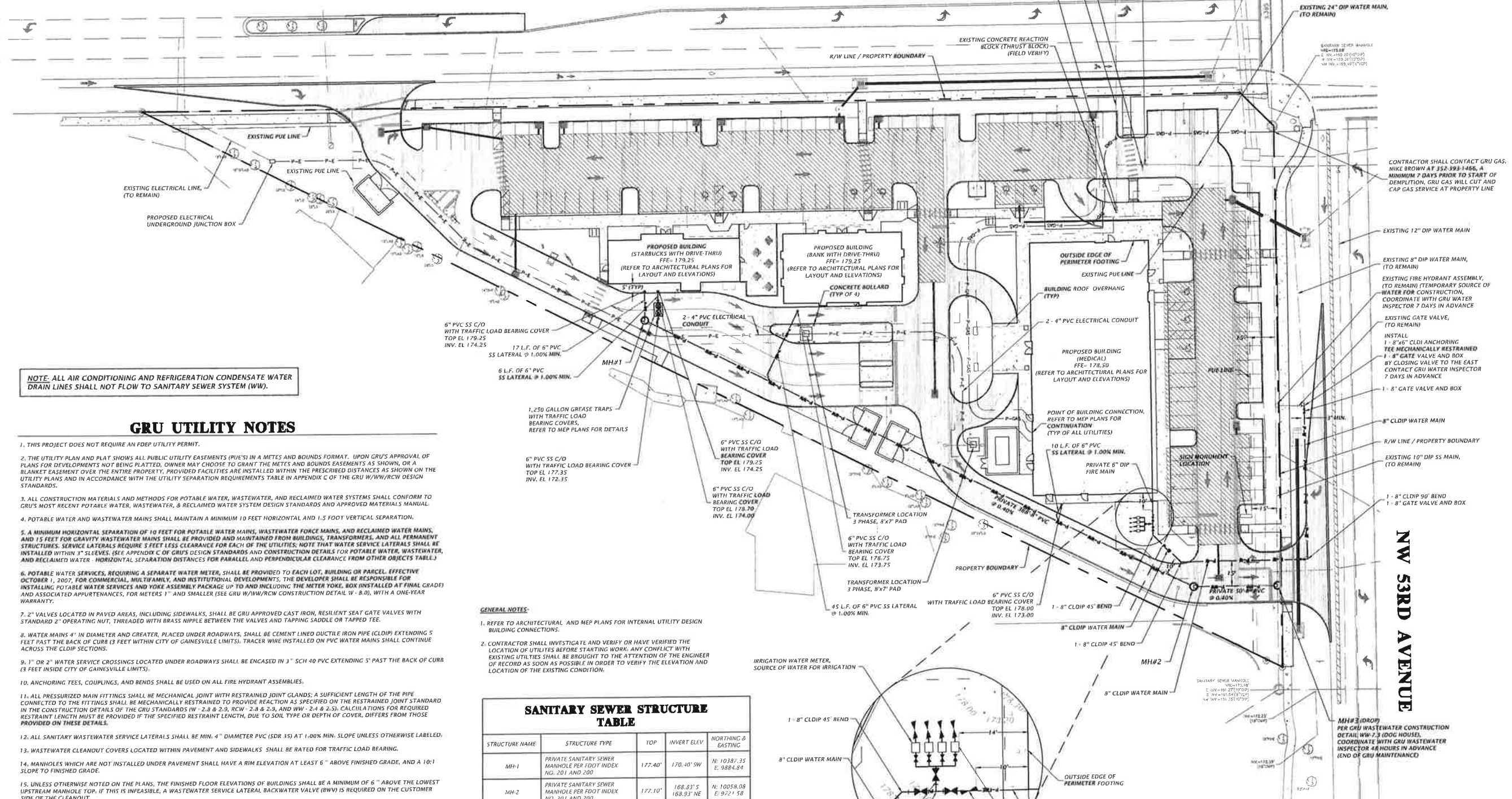
Professional Consultant

1"=30'
VERTICAL SCALE
AS SHOWN ON SHEET

CONTRACTOR SHALL CONTACT GRU GAS, MIKE BROWN AT 352-393-1486, A MINIMUM 7 DAYS PRIOR TO START OF DEMOLITION. GRU GAS WILL CUT AND CAP GAS SERVICE AT PROPERTY LINE.

CLIENT: BOOS DEVELOPMENT GROUP
PROJECT: NW 43RD STREET AND NW 53RD AVENUE RETAIL DEVELOPMENT
SHEET TITLE: UTILITY PLAN
DATE: 12-15-04
PROJECT NO.: 0490

FL PE No. 58206
SHEET NO.:
C3.00



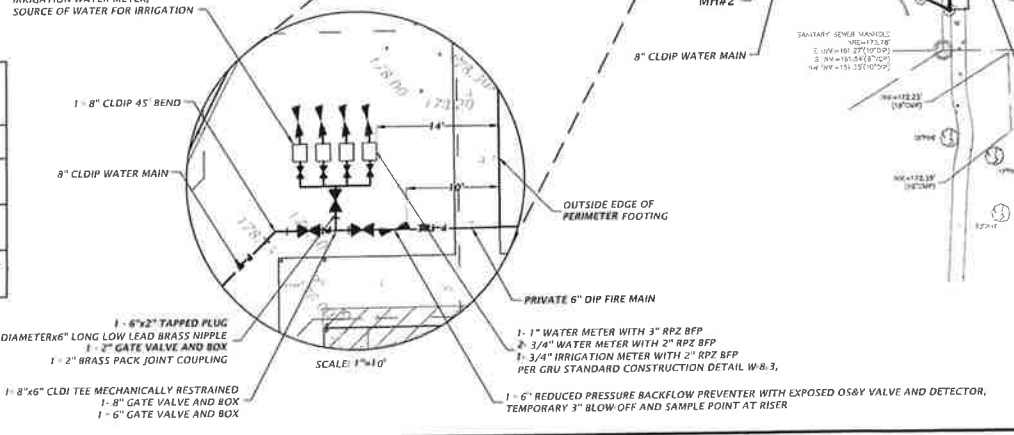
NOTE: ALL AIR CONDITIONING AND REFRIGERATION CONDENSATE WATER DRAIN LINES SHALL NOT FLOW TO SANITARY SEWER SYSTEM (WW).

GRU UTILITY NOTES

1. THIS PROJECT DOES NOT REQUIRE AN FDEP UTILITY PERMIT.
2. THE UTILITY PLAN AND PLAT SHOWS ALL PUBLIC UTILITY EASEMENTS (PUE'S) IN A METES AND BOUNDS FORMAT. UPON GRU'S APPROVAL OF PLANS FOR DEVELOPMENTS NOT BEING PLATTED, OWNER MAY CHOOSE TO GRANT THE METES AND BOUNDS EASEMENTS AS SHOWN, OR A BLANKET EASEMENT OVER THE ENTIRE PROPERTY. PROVIDED FACILITIES ARE INSTALLED WITHIN THE PRESCRIBED DISTANCES AS SHOWN ON THE UTILITY PLANS AND IN ACCORDANCE WITH THE UTILITY SEPARATION REQUIREMENTS TABLE IN APPENDIX C OF THE GRU W/W/R/W DESIGN STANDARDS.
3. ALL CONSTRUCTION MATERIALS AND METHODS FOR POTABLE WATER, WASTEWATER, AND RECLAIMED WATER SYSTEMS SHALL CONFORM TO GRU'S MOST RECENT POTABLE WATER, WASTEWATER, & RECLAIMED WATER SYSTEM DESIGN STANDARDS AND APPROVED MATERIALS MANUAL.
4. POTABLE WATER AND WASTEWATER MAINS SHALL MAINTAIN A MINIMUM 10 FEET HORIZONTAL AND 1.5 FOOT VERTICAL SEPARATION.
5. A MINIMUM HORIZONTAL SEPARATION OF 10 FEET FOR POTABLE WATER MAINS, WASTEWATER FORCE MAINS, AND RECLAIMED WATER MAINS, AND 15 FEET FOR GRAVITY WASTEWATER MAINS SHALL BE PROVIDED AND MAINTAINED FROM BUILDINGS, TRANSFORMERS, AND ALL PERMANENT STRUCTURES. SERVICE LATERALS REQUIRE 5 FEET LESS CLEARANCE FOR EACH OF THE UTILITIES; NOTE THAT WATER SERVICE LATERALS SHALL BE INSTALLED WITHIN 3" SLEEVES. (SEE APPENDIX C OF GRU'S DESIGN STANDARDS AND CONSTRUCTION DETAILS FOR POTABLE WATER, WASTEWATER, AND RECLAIMED WATER - HORIZONTAL SEPARATION DISTANCES FOR PARALLEL AND PERPENDICULAR CLEARANCE FROM OTHER OBJECTS TABLE.)
6. POTABLE WATER SERVICES, REQUIRING A SEPARATE WATER METER, SHALL BE PROVIDED TO EACH LOT, BUILDING OR PARCEL EFFECTIVE OCTOBER 1, 2007. FOR COMMERCIAL, MULTIFAMILY, AND INSTITUTIONAL DEVELOPMENTS, THE DEVELOPER SHALL BE RESPONSIBLE FOR INSTALLING POTABLE WATER SERVICES AND YOKE ASSEMBLY PACKAGE UP TO AND INCLUDING THE METER YOKE, BOX (INSTALLED AT FINAL GRADE) AND ASSOCIATED APPURTENANCES, FOR METERS 1" AND SMALLER (SEE GRU W/W/R/W CONSTRUCTION DETAIL W-8.0), WITH A ONE-YEAR WARRANTY.
7. 2" VALVES LOCATED IN PAVED AREAS, INCLUDING SIDEWALKS, SHALL BE GRU APPROVED CAST IRON, RESILIENT SEAT GATE VALVES WITH STANDARD 2" OPERATING NUT, THREADED WITH BRASS NIPPLE BETWEEN THE VALVES AND TAPPING SADDLE OR TAPPED TEE.
8. WATER MAINS 4" IN DIAMETER AND GREATER, PLACED UNDER ROADWAYS, SHALL BE CEMENT LINED DUCTILE IRON PIPE (CLDIP) EXTENDING 5 FEET PAST THE BACK OF CURB (3 FEET WITHIN CITY OF GAINESVILLE LIMITS). TRACER WIRE INSTALLED ON PVC WATER MAINS SHALL CONTINUE ACROSS THE CLDIP SECTIONS.
9. 1" OR 2" WATER SERVICE CROSSINGS LOCATED UNDER ROADWAYS SHALL BE ENCASED IN 3" SCH 40 PVC EXTENDING 5' PAST THE BACK OF CURB (3 FEET INSIDE CITY OF GAINESVILLE LIMITS).
10. ANCHORING TEES, COUPLINGS, AND BENDS SHALL BE USED ON ALL FIRE HYDRANT ASSEMBLIES.
11. ALL PRESSURIZED MAIN FITTINGS SHALL BE MECHANICAL JOINT WITH RESTRAINED JOINT GLANDS; A SUFFICIENT LENGTH OF THE PIPE CONNECTED TO THE FITTINGS SHALL BE MECHANICALLY RESTRAINED TO PROVIDE REACTION AS SPECIFIED ON THE RESTRAINED JOINT STANDARD IN THE CONSTRUCTION DETAILS OF THE GRU STANDARDS (W-2.8 & 2.9, RCW-2.8 & 2.9, AND WW-2.4 & 2.5). CALCULATIONS FOR REQUIRED RESTRAINT LENGTH MUST BE PROVIDED IF THE SPECIFIED RESTRAINT LENGTH, DUE TO SOIL TYPE OR DEPTH OF COVER, DIFFERS FROM THOSE PROVIDED ON THESE DETAILS.
12. ALL SANITARY WASTEWATER SERVICE LATERALS SHALL BE MIN. 4" DIAMETER PVC (SDR 35) AT 1.00% MIN. SLOPE UNLESS OTHERWISE LABELED.
13. WASTEWATER CLEANOUT COVERS LOCATED WITHIN PAVEMENT AND SIDEWALKS SHALL BE RATED FOR TRAFFIC LOAD BEARING.
14. MANHOLES WHICH ARE NOT INSTALLED UNDER PAVEMENT SHALL HAVE A RIM ELEVATION AT LEAST 6" ABOVE FINISHED GRADE, AND A 10:1 SLOPE TO FINISHED GRADE.
15. UNLESS OTHERWISE NOTED ON THE PLANS, THE FINISHED FLOOR ELEVATIONS OF BUILDINGS SHALL BE A MINIMUM OF 6" ABOVE THE LOWEST UPSTREAM MANHOLE TOP. IF THIS IS INFEASIBLE, A WASTEWATER SERVICE LATERAL BACKWATER VALVE (BWV) IS REQUIRED ON THE CUSTOMER SIDE OF THE CLEANOUT.
16. WHEN A POTABLE OR RECLAIMED WATER MAIN, OR A WASTEWATER FORCE MAIN IS ROUTED WITHIN 10 FT. OF AN ELECTRIC TRANSFORMER, A 20 FT. LENGTH OF DIP SHALL BE CENTERED ON THE TRANSFORMER WITH MECHANICAL RESTRAINT AT EACH END. NO FITTINGS OR VALVES SHALL OCCUR WITHIN 10 FT. OF THE NEAREST EDGE OF THE TRANSFORMER. A MINIMUM CLEARANCE OF 3' SHALL BE MAINTAINED BETWEEN THE MAIN AND THE TRANSFORMER.
17. WATER FOR IRRIGATION USE TO BE PROVIDED FROM PRIVATE WATER SYSTEM.

- GENERAL NOTES:**
1. REFER TO ARCHITECTURAL AND MEP PLANS FOR INTERNAL UTILITY DESIGN BUILDING CONNECTIONS.
 2. CONTRACTOR SHALL INVESTIGATE AND VERIFY OR HAVE VERIFIED THE LOCATION OF UTILITIES BEFORE STARTING WORK. ANY CONFLICT WITH EXISTING UTILITIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER OF RECORD AS SOON AS POSSIBLE IN ORDER TO VERIFY THE ELEVATION AND LOCATION OF THE EXISTING CONDITION.

SANITARY SEWER STRUCTURE TABLE				
STRUCTURE NAME	STRUCTURE TYPE	TOP	INVERT ELEV.	NORTHING & EASTING
MH-1	PRIVATE SANITARY SEWER MANHOLE PER FDOT INDEX NO. 201 AND 200	177.40'	170.40' SW	N: 10382.35 E: 9884.84
MH-2	PRIVATE SANITARY SEWER MANHOLE PER FDOT INDEX NO. 201 AND 200	177.10'	168.33' S 168.33' NE	N: 10058.08 E: 9721.58
MH-3	SANITARY SEWER MANHOLE PER FDOT INDEX NO. 101 AND 200 (DOG HOUSE) (DROPP)	174.70'	161.09' E 168.09' W 165.97' N 61.19' N (DROPP)	N: 10007.59 E: 9721.83



MH#3 (DROPP) PER GRU WASTEWATER CONSTRUCTION DETAIL WW-2.3 (DOG HOUSE). COORDINATE WITH GRU WASTEWATER INSPECTOR 48 HOURS IN ADVANCE (END OF GRU MAINTENANCE)



Buford Davis + Associates
 landscape architects site planners
 2406 NW 43rd Street
 Gainesville, Florida 32606
 352.335.1898 tel
 352.373.6407 fax
 Certificate of Authorization No. LC26000527

BOOS DEVELOPMENT
 NW 43RD STREET & NW 53RD AVENUE
 GAINESVILLE, FL

CONSULTANT:

SEAL:

PROJECT NO: 16-020
 DRAWN BY: CMT
 CHECKED BY: CEM
 DATE: 05 APR 2016
 REVISIONS:
 17 MAY 2016

DRAWING SCALE:

DRAWING TITLE:
LANDSCAPE PLAN

ISSUED FOR:
 SITE PLAN REVIEW

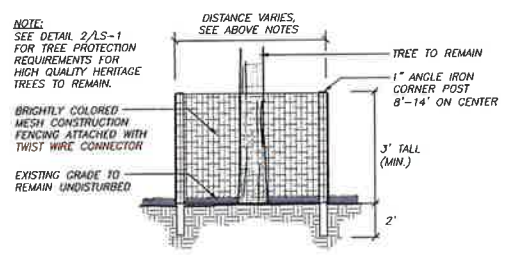
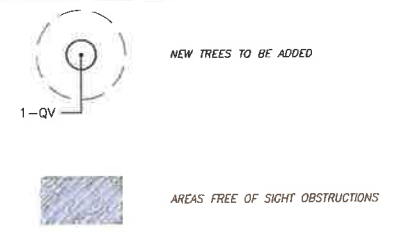
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LS-1

SHEET: 1 OF 2

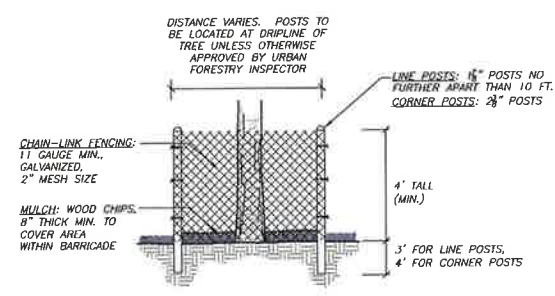
PLANT SCHEDULE

TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE
BN	4	BETULA NIGRA 'DURA HEAT'	DURA HEAT RIVER BIRCH	30 GAL, 12' HT, 42" SPR, 2" CAL, MULTI-TRUNK (3 TRUNKS)
LT	13	LIRIODENDRON TULIPIFERA	TULIP TREE	65 GAL, 13' HT, 5' SPR, 3.5" CAL
MGD	4	MAGNOLIA GRANDIFLORA 'DD BLANCHARD'	DD BLANCHARD MAGNOLIA	30 GAL, 12' HT, 42" SPR, 2" CAL
QVP	13	QUERCUS VIRGINIANA 'PARK SIDE'	PARK SIDE LIVE OAK	30 GAL, 12' HT, 54" SPR, 2" CAL
QVPL	10	QUERCUS VIRGINIANA 'PARK SIDE'	PARK SIDE LIVE OAK	65 GAL, 14' HT, 5.5" SPR, 3.5" CAL
UPE	12	ULMUS PARVIFOLIA 'ALLEE'	ALLEE ELM	30 GAL, 10' HT, 4" SPR, 2" CAL
SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE
IDB	249	ILEX CORNUTA 'DWARF BURFORDI'	DWARF BURFORD HOLLY	3 GAL, 24" HT, 16" SPR
PM	151	PODOCARPUS MACROPHYLLUS 'PRINGLES DWARF'	PRINGLES DWARF PODOCARPUS	3 GAL, 36" HT, 9" SPR
SOD/SEED		BOTANICAL NAME	COMMON NAME	SIZE
SOD		PASPALUM NOTATUM 'ARGENTINE'	BAHIA GRASS	WEED FREE AND SAND GROWN SOD

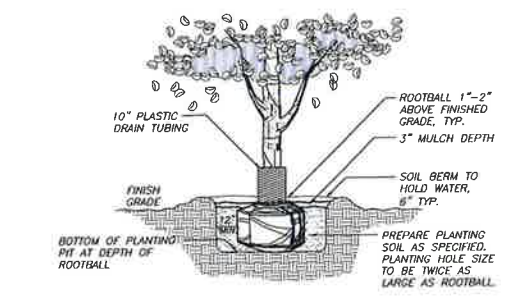
LEGEND



1 STANDARD TREE PROTECTION DETAIL - REGULATED, NON-HIGH QUALITY HERITAGE
 SCALE: N.T.S.

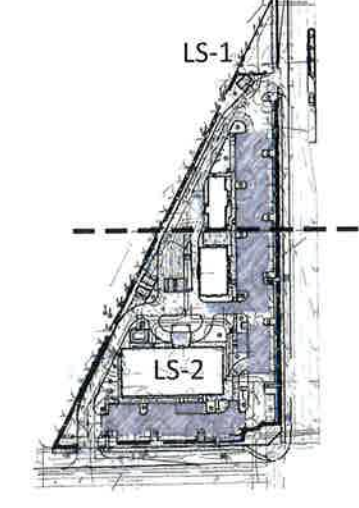


2 HIGH-QUALITY HERITAGE TREE PROTECTION DETAIL
 SCALE: N.T.S.



3 TREE PLANTING DETAIL
 SCALE: N.T.S.

SHEET REFERENCE MAP
 N.T.S.



GENERAL NOTES
 ALL ASPHALT, LIMESTONE, AND CONSTRUCTION DEBRIS TO BE REMOVED FROM PLANTING BEDS AND AREAS TO BE SOODED PRIOR TO LANDSCAPE INSTALLATION. PLANTING DEPTH OF SOIL IN SUCH AREAS SHOULD BE AT LEAST 3'. IF FILL MUST BE ADDED, IT MUST BE FLORIDA CLEAN DEEP FILL (FREE OF WEED SEEDS) WITH pH 5.5-6.5.
 ALL TREES TO BE A MINIMUM SEVEN (7) FEET IN HEIGHT AND TWO (2) INCH CALIPER, OR LARGER, UNLESS OTHERWISE SPECIFIED BY THE LANDSCAPE ARCHITECT.
 ALL TREES TO BE PLANTED 1"-2" ABOVE FINISHED GRADE.
 ALL LANDSCAPED AREAS SHALL BE MULCHED WITH 3" THICKNESS OF MULCH. PINE BARK MULCH SHALL BE USED IN ALL AREAS.
 TREES OUTSIDE OF MULCHED LANDSCAPED BEDS TO BE PROTECTED FROM MOWER AND STRING TRIMMER DAMAGE WITH 10" OF PLASTIC DRAIN TUBE.
 LEADER SHOOTS AND MAIN STRUCTURAL LIMBS OF TREES WILL NOT BE TOPPED OR PRUNED. TREES TO BE STAKED AS NEEDED. GUYLINES TO BE NON-SYNTHETIC BIODEGRADABLE MATERIAL.
 OWNER SHALL BE RESPONSIBLE FOR (1) THE SURVIVAL OF THE LANDSCAPING ELEMENTS AND (2) REMOVAL OF ALL STAKING SYSTEMS WITHIN ONE YEAR. TREES WILL BE STAKED ONLY IF NECESSARY, AND IF STAKED, BIODEGRADABLE TWINE WILL BE USED.
 CALL THE URBAN FORESTRY INSPECTOR FOR A BARRICADE INSPECTION AT 352-393-8188 BEFORE CLEARING AND GRUBBING WORK BEGINS.

INVASIVE EXOTIC PLANT MATERIAL
 ALL INVASIVE PLANT SPECIES TO BE REMOVED FROM SITE PRIOR TO ISSUANCE OF THE CERTIFICATE OF OCCUPANCY.

GRASSING
 ALL DISTURBED AND UNPAVED AREAS TO BE GRASSED WITH SOD THAT IS FREE OF NOXIOUS WEEDS INCLUDING TROPICAL SODA APPLE OR SHALL BE SEEDED AND MULCHED. SEE CIVIL SITE PLANS FOR ADDITIONAL RELATED INFORMATION.

PLANT MATERIAL
 ALL PLANT MATERIAL TO BE FLORIDA NO.1 OR BETTER, GRADED IN ACCORDANCE WITH THE STATE OF FLORIDA DEPARTMENT OF AGRICULTURE, DIVISION OF PLANT INDUSTRY'S 'GRADING AND STANDARDS FOR NURSERY PLANTS'. CALL THE CITY URBAN FORESTRY INSPECTOR AT 352-393-8188 AND LANDSCAPE ARCHITECT PRIOR TO PURCHASING ANY PLANT MATERIAL FOR AN ON SITE INSPECTION AND REVIEW OF LANDSCAPE PLAN.

IRRIGATION
 LANDSCAPE IRRIGATION TO BE PROVIDED BY AUTOMATIC IRRIGATION SYSTEM. BUBBLERS SHALL BE INSTALLED BY EACH TREE AS PART OF THE IRRIGATION SYSTEM.

T.M.P.A.
 SITE IS LOCATED IN ZONE B OF THE TRANSPORTATION MOBILITY PROGRAM AREA.

TREE PROTECTION NOTES

- REGULATED TREES TO REMAIN THAT ARE WITHIN 50 FEET OF ANY CONSTRUCTION ACTIVITY OR STORAGE OF EQUIPMENT OR MATERIALS SHALL BE PROTECTED BY A TREE PROTECTION BARRIAR.
- TREE BARRIAR SHALL BE PLAINLY VISIBLE AND SHALL CREATE A CONTINUOUS BOUNDARY AROUND TREES TO PREVENT ENCROACHMENT BY MACHINERY. NO CONSTRUCTION EQUIPMENT, MATERIALS, SUPPLIES, FUELS, OR CHEMICALS SHALL BE STORED WITHIN THE TREE BARRIAR LIMITS AT ANY TIME.
- BARRIAR SHALL BE PLACED AT OR OUTSIDE THE DRIP LINE FOR HERITAGE AND CHAMPION TREES AND ALL REGULATED PINE AND PALM TREES. FOR ALL OTHER REGULATED TREES TO REMAIN, TREE BARRIAR SHALL BE LOCATED AT TWO-THIRDS THE AREA OF THE TREE DRIPLINE OR AT THE ROOT PLATE AT A MINIMUM AND WITH CITY URBAN FORESTRY INSPECTOR APPROVAL.
- NO GRADE CHANGES SHALL BE MADE WITHIN THE PROTECTIVE BARRIAR ZONES WITHOUT PRIOR APPROVAL OF THE CITY MANAGER OR DESIGNEE.
- TREE BARRIAR SHALL REMAIN IN PLACE AND IN WORKING ORDER THROUGHOUT CONSTRUCTION. BARRIAR SHALL BE REMOVED ONCE MAJOR CONSTRUCTION ACTIVITIES ARE COMPLETE AND LANDSCAPE INSTALLATION BEGINS. LANDSCAPE PREPARATION WITHIN THE PROTECTED AREAS SHALL BE LIMITED TO SHALLOW DISKING LIMITED TO A DEPTH OF FOUR (4) INCHES UNLESS OTHERWISE APPROVED BY CITY MANAGER.
- A LAYER OF WOOD CHIPS AT LEAST EIGHT (8) INCHES THICK SHALL COVER THE SOIL WITHIN THE TREE BARRIAR AREA.
- WHERE ROOTS GREATER THAN ONE (1) INCH ARE DAMAGED OR EXPOSED, THEY SHALL BE CUT CLEANLY AND RE-COVERED WITH SOIL WITHIN ONE HOUR OF DAMAGE OR EXPOSURE.
- CONTRACTOR SHALL TRIM ONLY THOSE EXISTING TREES AS NOTED ON THE PLAN OR AS APPROVED BY OWNER'S REPRESENTATIVE AND LANDSCAPE ARCHITECT. TRIMMING/PRUNING SHALL BE DONE IN ACCORDANCE WITH THE MOST CURRENT VERSION OF THE AMERICAN NATIONAL STANDARD FOR TREE CARE OPERATIONS TREE, SHRUB, AND OTHER WOODY PLANT MAINTENANCE (ANSI Z60) AND TRUNING, TRIMMING, REPAIRING, MAINTAINING, AND REMOVING TREES, AND CUTTING BRUSH SAFETY REQUIREMENTS' (ANSI Z133). REMOVE NO MORE THAN 25% OF THE CROWN AT ONE TIME. ON YOUNG TREES, REMOVE NO MORE THAN 33% OF THE TRUNK BARK OR BRANCHES. DISCUSS PROPOSED TRIMMING ACTIVITIES WITH OWNER'S REPRESENTATIVE AND CITY'S URBAN FORESTRY INSPECTOR PRIOR TO ACTIVITIES/REMOVAL.
- STANDARD TREE PROTECTION BARRIAR FOR REGULATED TREES THAT ARE NOT HIGH-QUALITY HERITAGE - SEE DETAIL 1/LS-1. BARRIAR TO BE A MINIMUM OF THREE (3) FEET TALL AND CONSTRUCTED OF EITHER 2X4 WOODEN CORNER POSTS BURIED TO ONE FOOT DEPTH WITH AT LEAST TWO COURSES OF 1X4 WOODEN SIDE SLATS WITH COLORED FLAGGING-COLORED MESH ATTACHED OR CONSTRUCTED OF ONE-INCH ANGLE IRON CORNER POSTS WITH BRIGHTLY COLORED MESH CONSTRUCTION FENCING ATTACHED. SEE NOTE 10 FOR ADDITIONAL REQUIREMENTS FOR HIGH-QUALITY HERITAGE TREE BARRIAR.
- ADDITIONAL BARRIAR REQUIREMENTS FOR HIGH QUALITY HERITAGE TREES TO REMAIN - SEE DETAIL 2/LS-1. HIGH QUALITY HERITAGE TREES SHALL BE PROTECTED BY GALVANIZED CHAIN LINK FENCING A MINIMUM OF 48" HIGH, 11-GAUGE WIRE, AND 2" MESH SIZE SECURED WITH 1-7/8" LINE POSTS NO FURTHER THAN 10 FT. APART AND SECURED AT A DEPTH OF THREE (3) FEET BELOW THE SOIL LINE. BARRIAR CORNERS SHALL BE SECURED WITHIN 2-3/8" POSTS SECURED TO A DEPTH OF FOUR (4) FEET BELOW THE SOIL LINE.
- CALL PLANNING SERVICES AT 352-393-8188 TO SCHEDULE A BARRICADE INSPECTION BEFORE ANY CLEARING AND GRUBBING WORK BEGINS.

TREE MITIGATION CALCULATIONS Sec. 30-254(c)(4)	
CATEGORY	MITIGATION CALCULATIONS
TREE APPRAISED VALUE (T.A.V.)	TOTAL TREES IN CATEGORY TO BE MITIGATED: 2 TREES
	MAXIMUM APPRAISED TREES: 5.38 TREES
	MAXIMUM 3 TREES PER ACRE
	TOTAL SITE AREA: 2.69 ACRES
	TREES BEYOND MAXIMUM, TO BE MITIGATED AT INCH FOR INCH REPLACEMENT (SEE BELOW): 0 TREES
APPRAISED TREES:	
24" LIVE OAK	\$9,947.52
20" LIVE OAK	\$11,624.92
TOTAL	\$21,622.04
INCH-FOR-INCH REPLACEMENT	T.A.V. TREES BEYOND MAXIMUM
	N/A
	HIGH-QUALITY HERITAGE TREES IN LESS THAN FAIR CONDITION
	N/A
	NON-HIGH QUALITY HERITAGE TREES IN FAIR OR BETTER CONDITION
	25" SOUTHERN RED CEDAR..... 25"
	23" SOUTHERN RED CEDAR..... 23"
	22" SOUTHERN RED CEDAR..... 22"
	25" SOUTHERN RED CEDAR..... 25"
	26" SOUTHERN RED CEDAR..... 26"
	25" SOUTHERN RED CEDAR..... 25"
	29" SOUTHERN RED CEDAR..... 29"
	29" SOUTHERN RED CEDAR..... 29"
	35" SOUTHERN RED CEDAR..... 35"
	23" SOUTHERN RED CEDAR..... 23"
	23" SOUTHERN RED CEDAR..... 23"
	TOTAL INCHES REQUIRED..... 285"
2:1 REPLACEMENT	TOTAL TREES TO BE MITIGATED AT 2:1 REPLACEMENT..... 21 TREES
	TOTAL TREES REQUIRED FOR MITIGATION @ 2:1 REPLACEMENT..... 42 TREES
	TOTAL INCHES REQUIRED @ 2"/TREE..... 84"
TOTAL TREE MITIGATION	TOTAL MITIGATION INCHES REQUIRED..... 369"
	TOTAL MITIGATION PROVIDED..... 146.5"
	33 @ 2" = 66"
	23 @ 3.5" = 80.5"
	56 TREES FOR A TOTAL OF 146.5"
	TOTAL MITIGATION INCHES DEFICIT..... 222.5"
	TOTAL MITIGATION TREES REQUIRED @ 2"/TREE..... 111.25 TREES
TOTAL MITIGATION PAYMENT	TOTAL MITIGATION PAYMENT:
	TREE APPRAISALS TOTAL..... \$21,622.04
	DEFICIT PAYMENT (\$100/TREE)..... \$11,125.00
	TOTAL PAYMENT REQUIRED..... \$32,747.04
	* NOTE: TREE MITIGATION PAYMENT SHALL BE MADE PRIOR TO THE TIME THAT THE DEVELOPMENT ORDER IS ISSUED OR AT THE TIME THAT THE SITE WORK PERMIT IS GRANTED FOR THE DEVELOPMENT FROM THE BUILDING DEPARTMENT.

PLOTDATE: 05/17/2016 9:28 AM USER: CAELI TOAR
 FILENAME: Z:\CLIENTS\16-020 BOOS DEVELOPMENT (43RD & 53RD RETAIL) (43RD & 53RD RETAIL) (05-17-SPR2) (16-020-BOOS-BASE.DWG)



Buford Davis + Associates
landscape architects site planners
2406 NW 43rd Street
Gainesville, Florida 32606
352.335.1896 tel
352.373.6407 fax
Certificate of Authorization No. LC26000527

BOOS DEVELOPMENT

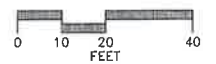
NW 43RD STREET & NW 53RD AVENUE
GAINESVILLE, FL

CONSULTANT:

SEAL:

PROJECT NO:	16-020
DRAWN BY:	CMT
CHECKED BY:	CEM
DATE:	05 APR 2016
REVISIONS:	
	17 MAY 2016

DRAWING SCALE:

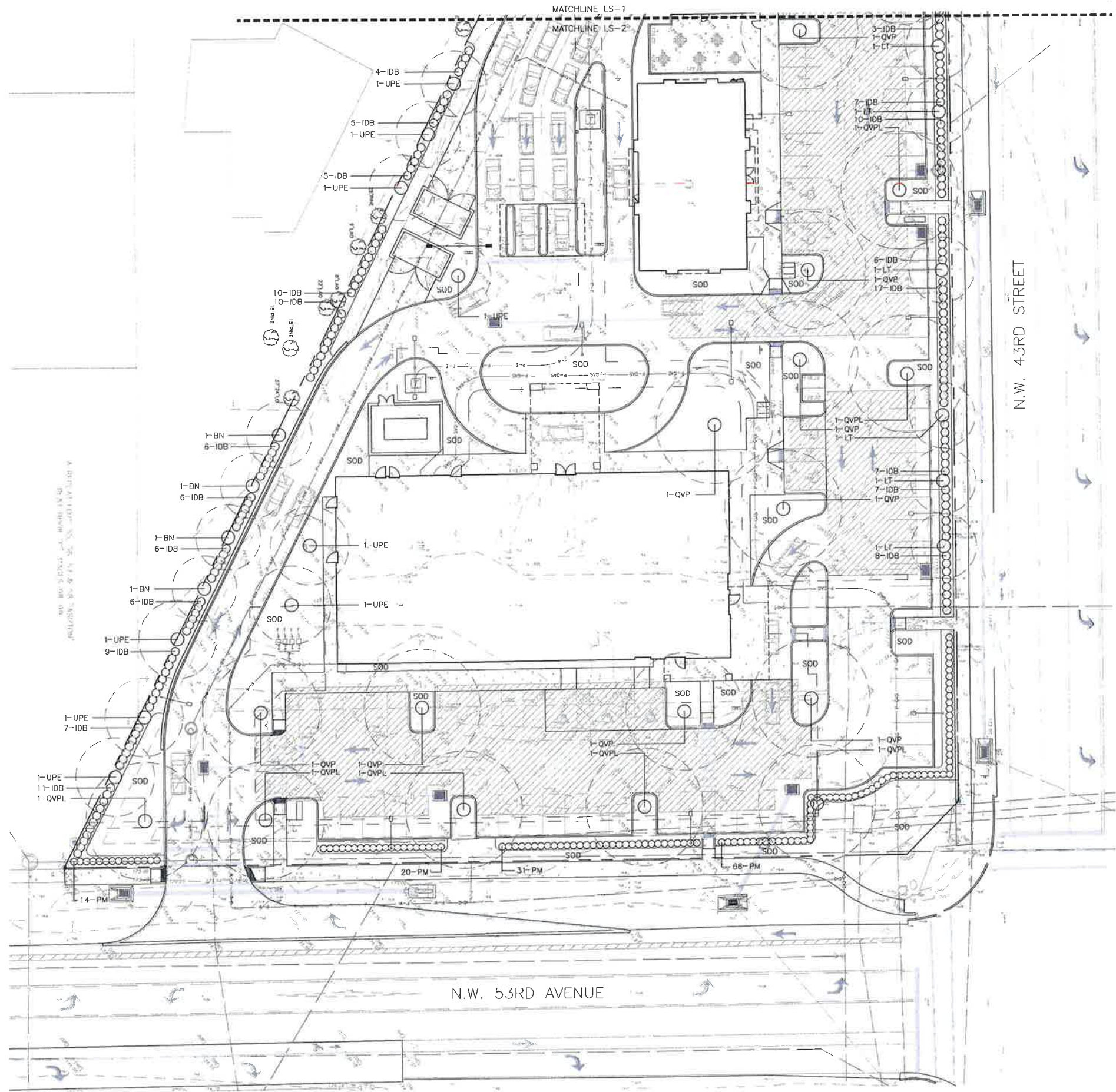
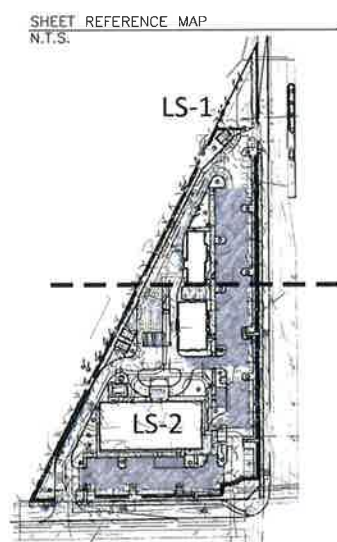


DRAWING TITLE:
LANDSCAPE PLAN

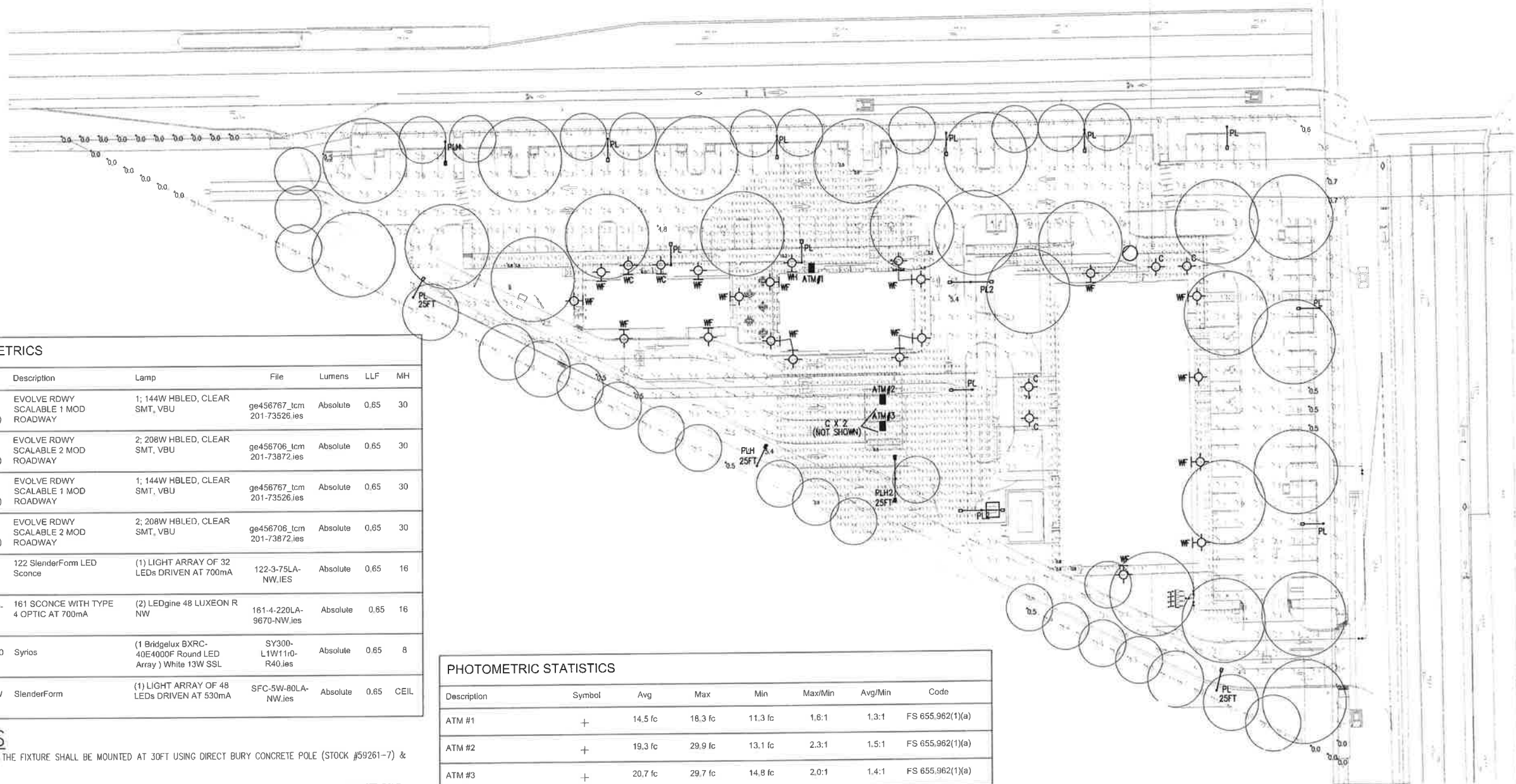
ISSUED FOR:
SITE PLAN REVIEW

DRAWING NUMBER:
LS-2

SHEET: 2 OF 2



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LUMINAIRE SCHEDULE FOR PHOTOMETRICS

Symbol	Label	Qty	Manufacturer	Catalog Number	Description	Lamp	File	Lumens	LLF	MH
PL	PL	12	GE (BY GRU)	ERS10J2E15402 GRAYBLT (STOCK #58121-6)	EVOLVE RDWY SCALABLE 1 MOD ROADWAY	1; 144W HBLED, CLEAR SMT, VBU	ge456767_tcm 201-73526.ies	Absolute	0.65	30
PLH	PLH	2	GE (BY GRU)	ERS2_F3E1140 GRAYBLT (STOCK #58131-3)	EVOLVE RDWY SCALABLE 2 MOD ROADWAY	2; 208W HBLED, CLEAR SMT, VBU	ge456706_tcm 201-73872.ies	Absolute	0.65	30
PL2	PL2	2	GE (BY GRU)	ERS10J2E15402 GRAYBLT (STOCK #58121-6)	EVOLVE RDWY SCALABLE 1 MOD ROADWAY	1; 144W HBLED, CLEAR SMT, VBU	ge456767_tcm 201-73526.ies	Absolute	0.65	30
PLH2	PLH2	1	GE (BY GRU)	ERS2_F3E1140 GRAYBLT (STOCK #58131-1)	EVOLVE RDWY SCALABLE 2 MOD ROADWAY	2; 208W HBLED, CLEAR SMT, VBU	ge456706_tcm 201-73872.ies	Absolute	0.65	30
WF	WF	17	GARDCO	122L-3-75LA-NW	122 SlenderForm LED Sconce	(1) LIGHT ARRAY OF 32 LEDs DRIVEN AT 700mA	122-3-75LA-NW.ies	Absolute	0.65	16
WH	WH	1	GARDCO	161-4-220LA-9670-NW	161 SCONCE WITH TYPE 4 OPTIC AT 700mA	(2) LEDgine 48 LUXEON R NW	161-4-220LA-9670-NW.ies	Absolute	0.85	16
WC	WC	2	LUMINIS	SY300-L1W11-R40	Syrios	(1) Bridgelux BXRC-40E4000F Round LED Array) White 13W SSL	SY300-L1W11r0-R40.ies	Absolute	0.65	8
C	C	6	GARDCO	SFC-5W-80LA-NW	SlenderForm	(1) LIGHT ARRAY OF 48 LEDs DRIVEN AT 530mA	SFC-5W-80LA-NW.ies	Absolute	0.65	CEIL

FIXTURE SCHEDULE NOTES

1. TYPE PL IS A POLE MOUNT, FULL CUT-OFF LED FIXTURE, THE FIXTURE SHALL BE MOUNTED AT 30FT USING DIRECT BURY CONCRETE POLE (STOCK #59261-7) & 10FT ARMS (STOCK #52573-1).
2. TYPES PL2 & PLH2 ARE POLE MOUNT, TWIN FULL CUT-OFF LED FIXTURES, THE FIXTURES SHALL BE MOUNTED AT 30FT USING A DIRECT BURY CONCRETE POLE (IEA- STOCK #11601-7) & 10FT ARMS (2EA- STOCK #52573-1).
3. LIGHT POLES ADJACENT TO PROPERTY LINE ABUTTING RESIDENTIAL USES SHALL BE MOUNTED AT 25FT USING DIRECT BURY CONCRETE POLES (STOCK #59262-5) & 10FT ARMS (STOCK #52573-1) AS NOTED.
4. TYPES WF & WH ARE WALL MOUNT, FULL CUT-OFF LED FIXTURES, THE FIXTURES SHALL BE MOUNTED AT 16FT.
5. TYPE WC IS A WALL MOUNT, FULL CUT-OFF LED CYLINDER FIXTURE, THE FIXTURE SHALL BE MOUNTED AT 8FT.
6. TYPE C IS A CEILING SURFACE MOUNT, FULL CUT-OFF LED FIXTURE, THE FIXTURE SHALL BE MOUNTED ON THE UNDERSIDE OF THE CANOPY.
7. DUE TO THE FULL CUT-OFF NATURE OF ALL PROPOSED FIXTURES, LIGHT LEVELS AT 5FT ABOVE THE FIXTURES ARE OFC BY DEFINITION.

PHOTOMETRIC NARRATIVE AND WAIVER REQUEST

THE FOLLOWING IS INTENDED TO SERVE AS A PHOTOMETRIC NARRATIVE AND WAIVER REQUEST FOR THIS PROJECT.

THE PROJECT CONSISTS OF NEW RETAIL, RESTAURANT, AND BANK BUILDINGS, ASSOCIATED PARKING, AND WALKWAYS. THE RESTAURANT AND BANKING USES HAVE DRIVE-THROUGH AREAS, AND THE RETAIL USE HAS TWO COVERED CANOPIES. THE SITE IS BORDERED ON THE EAST & SOUTH SIDES BY PUBLIC RIGHT-OF-WAY, AND ON THE WEST SIDE BY A COMMERCIAL USES.

THE BANK BUILDING INCLUDES 3 ATM MACHINES, FOR WHICH SPECIFIC LIGHTING REQUIREMENTS EXIST WITHIN THE STATE OF FLORIDA STATUTES, SECTION 655.962. THIS LIGHTING IS EXEMPT FROM THE REQUIREMENTS OF 30-344 AND IS INCLUDED HERE FOR INFORMATIONAL PURPOSES ONLY. ALL LIGHT LEVELS SHOWN INCLUDE THE CONTRIBUTIONS OF THE ATM LIGHTING, HOWEVER, LIGHT TRESPASS VALUES WITHIN AND ADJACENT TO THE ATM LIGHTING ZONES HAVE BEEN OMITTED DUE TO THE CONFLICTING REQUIREMENTS.

THE PARKING LOT WILL BE SERVED BY POLE MOUNT, FULL CUT-OFF LED FIXTURES PROVIDED BY GRU. THESE FIXTURES SHALL BE MOUNTED AT 30FT. GRU CANNOT PROVIDE THE AUTOMATIC SHUT-OFF REQUIRED BY 30-344(e)(4)(i), THEREFORE A WAIVER TO THAT REQUIREMENT IS REQUESTED. THE FIXTURES WILL OPERATE DUSK-TO-DAWN IN ACCORDANCE WITH 30-344(d)(1)(e).

THE BUILDING WALKWAYS WILL BE SERVED BY WALL MOUNT, FULL CUT-OFF LED FIXTURES, THE MOUNTING HEIGHT FOR THESE FIXTURES SHALL BE 16FT AFG. THE FIXTURES WILL BE CONTROLLED WITH A PHOTOCELL AND SHUT OFF WITHIN 1 HR OF THE CLOSE OF BUSINESS, WITH THE EXCEPTION OF LIGHTS IN THE ATM ZONES.

THE COVERED DRIVEWAY AREAS WILL BE SERVED BY CEILING SURFACE MOUNT, FULL CUT-OFF LED FIXTURES. THE FIXTURES WILL BE CONTROLLED WITH A PHOTOCELL AND SHUT OFF WITHIN 1 HR OF THE CLOSE OF BUSINESS, WITH THE EXCEPTION OF LIGHTS IN THE ATM ZONES.

OVERALL, THE PROPOSED LIGHTING WILL PROVIDE A SAFE, SECURE ENVIRONMENT FOR BUILDING EMPLOYEES & PATRONS. THE LIGHTING WILL NOT ADVERSELY IMPACT THE ENVIRONMENT OR NEIGHBORING PROPERTIES.

PHOTOMETRIC STATISTICS

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min	Code
ATM #1	+	14.5 fc	18.3 fc	11.3 fc	1.6:1	1.3:1	FS 655.962(1)(a)
ATM #2	+	19.3 fc	29.9 fc	13.1 fc	2.3:1	1.5:1	FS 655.962(1)(a)
ATM #3	+	20.7 fc	29.7 fc	14.8 fc	2.0:1	1.4:1	FS 655.962(1)(a)
WALK-UP ATM ZONE	+	4.8 fc	10.2 fc	2.0 fc	5.1:1	2.4:1	FS 655.962(1)(b) & (c)
DRIVE-UP ATM ZONE	+	4.9 fc	9.5 fc	2.0 fc	4.8:1	2.5:1	FS 655.962(1)(b) & (c)
DRIVEWAYS	+	2.1 fc	5.4 fc	0.5 fc	10.8:1	4.2:1	30-344(e)(4)(c)
PARKING AREAS	+	2.2 fc	4.8 fc	0.5 fc	9.6:1	4.4:1	30-344(e)(4)(c)
PROPERTY LINE- NW 43RD ST	+	0.2 fc	0.6 fc	0.0 fc	N / A	N / A	30-344(d)(3)(b)
PROPERTY LINE- NW 53RD AVE	+	0.2 fc	0.7 fc	0.0 fc	N / A	N / A	30-344(d)(3)(b)
PROPERTY LINE- NW SIDE	+	0.2 fc	0.5 fc	0.0 fc	N / A	N / A	30-344(d)(3)(b)
STARBUCKS ENTRY	+	2.7 fc	6.0 fc	0.8 fc	7.5:1	3.4:1	30-344(e)(3)
LARGE BLDG ENTRY	+	2.7 fc	7.9 fc	0.8 fc	9.9:1	3.4:1	30-344(e)(3)
CHASE ENTRY	+	3.8 fc	4.3 fc	3.2 fc	1.3:1	1.2:1	30-344(e)(3)

PHOTOMETRIC NOTES

ATM LIGHTING IS GOVERNED BY FLORIDA STATUTES #655.962 AND THEREFORE IS EXEMPT FROM THE REQUIREMENTS OF 30-344. THIS INCLUDES LIGHT TRESPASS ASSOCIATED WITH THE ATM LIGHTING ZONES.

GENERAL NOTES

1. HIGHLIGHTED POINTS REPRESENT MAXIMUM/MINIMUM VALUE FOR EACH AREA.
2. FIXTURES WILL BE CONTROLLED WITH PHOTOCELL AND OPERATE DUSK-TO-DAWN.
3. POLE LIGHT LOCATIONS HAVE BEEN COORDINATED WITH EXISTING AND PROPOSED TREES.

SITE PHOTOMETRIC PLAN
SCALE: 1" = 30'



Project Number	1627
Date	5/16/16
Designed By	KWH
Checked By	RJN

Rev	Date	Description

NIX ENGINEERING, INC.
CONSULTING ENGINEERS
2711 NW 6TH ST, STE B
GAINESVILLE, FLORIDA

BOOS DEVELOPMENT
SITE PHOTOMETRIC PLAN
NW 43RD ST & 53RD AVE
GAINESVILLE, FL

Drawing Number
E-1

WALL LEGEND

INDICATES 8" CMU WALL AT INTERIOR, PROVIDE 20 GA. METAL STUD FURRING AT 16" O.C. WITH 5/8" TYPE 'X' GMB AND R-19 BATT INSULATION AT CAVITY, ALL FULL HEIGHT TO ROOF DECK. GENERAL CONTRACTOR SHALL COORDINATE INSTALLATION OF GMB WITH TENANT'S ROUGH-IN. SEE NOTES THIS SHEET



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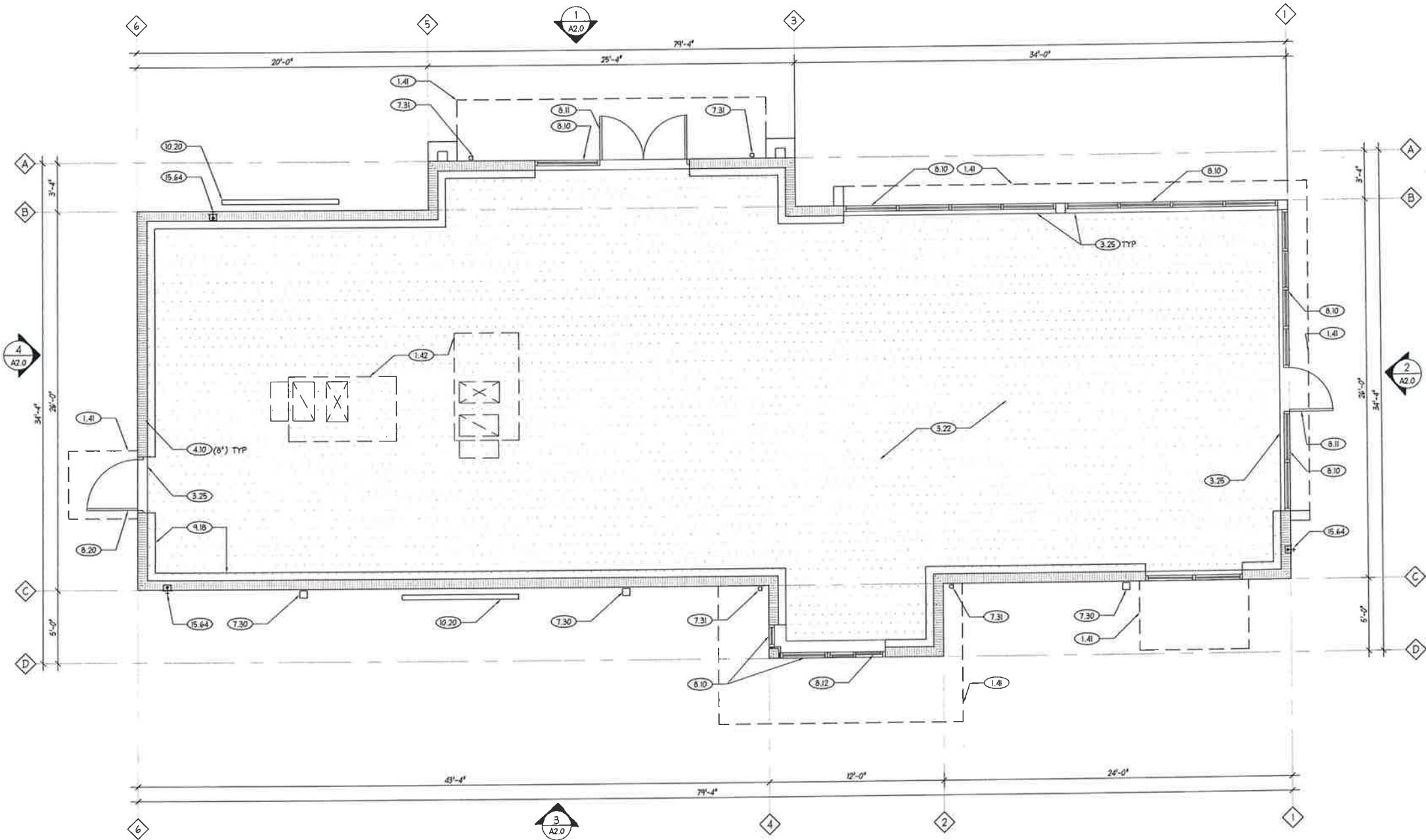


GUY F. FABER
 FL License No. AR0015323

GENERAL NOTES

- ALL DIMENSIONS ARE TO FINISHED FACE OF GMB PARTITIONS, FACE OF CMU & CENTERLINE OF COLUMNS U.O.N.
- SITE RELATED WORK:
 THE GC SHALL BE RESPONSIBLE FOR ALL UNDER SLAB PLUMBING AND ELECTRICAL AT THE BUILDING INTERIOR AND, ADDITIONALLY, SUB-GRADE ELECTRICAL AND FOUNDATIONS FOR EXTERIOR SITE DIRECTIONAL SIGNAGE, MENU-BOARDS, DETECTOR LOOP, HEIGHT RESTRICTION BAR AND RELATED SITE ITEMS WHICH ARE TO BE INSTALLED BY STARBUCKS AND/OR THEIR CONTRACTOR.
 THE GC SHALL OBTAIN A COMPLETE AND CURRENT SET OF STARBUCKS TENANT FINISH-OUT PLANS. THOSE DOCUMENTS SHALL GOVERN WHEN DETERMINING PRECISE LOCATIONS AND SPECIFICATIONS FOR ALL TENANT COMPONENTS INCLUDING, BUT NOT LIMITED TO, SITE AND DRIVE-THRU COMPONENTS, STUB-INS FOR PLUMBING FIXTURES, ELECTRICAL PANEL LOCATIONS AND STUB-INS FOR IN SLAB ELECTRICAL POWER. DIMENSIONS REQUIRED TO LOCATE THESE ITEMS HAVE BEEN INTENTIONALLY OMITTED IN THESE CONSTRUCTION DOCUMENTS TO AVOID POSSIBLE CONFLICT WITH TENANT PLANS.
- EXTERIOR CONCRETE:
 ALL EXTERIOR CONCRETE SHALL BE NATURAL GREY WITH MEDIUM BROOM FINISH RETOOL (WITH HAND EDGER) ALL CONTROL JOINTS AND EXPANSION JOINTS AFTER BROOM FINISHING TO ACHIEVE 'PICTURE FRAME' IN CONCRETE. THIS SHALL INCLUDE ALL ABUTMENTS TO WALLS, CURBS, COLUMNS, ETC.
- GYPSUM WALLBOARD:
 THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION OF 5/8" TYPE 'X' GYPSUM WALLBOARD OVER 6" 20 GA. METAL STUD FURRING AT 16" O.C. WITH R-19 BATT INSULATION AT THE CAVITY. SAID CONSTRUCTION SHALL BE PROVIDED AT THE INTERIOR PERIMETER OF THE EXTERIOR WALLS, U.O.N. AND SHALL EXTEND FULL HEIGHT TO ROOF DECK. ALL GMB SURFACES SHALL BE FINISHED TO A LEVEL 4 FINISH AND READY TO RECEIVE PAINT FINISH BY TENANT.
 THE GENERAL CONTRACTOR SHALL COORDINATE INSTALLATION OF GMB WITH STARBUCKS TENANT FINISH-OUT CONTRACTOR AND THEIR ROUGH-IN OF ELECTRICAL AND PLUMBING.

REFER TO SHEET A0.3 FOR KEYNOTE SCHEDULE



1 FLOOR PLAN
 SCALE: 1/4" = 1'-0"
 NORTH

**PRELIMINARY DRAWINGS
 NOT FOR CONSTRUCTION**

NO.	DATE	REVISION DESCRIPTIONS

**STARBUCKS
 SHELL CONSTRUCTION**
 5601 NW 43RD STREET
 GAINESVILLE, FL 32653

03.11.16
 date
 15076
 comm. no.
 FLOOR PLAN
 & NOTES

A1.0

REFER TO SHEET A0.3 FOR KEYNOTE SCHEDULE

PRIMARY AND EMERGENCY ROOF DRAIN CALCULATIONS

FPC - PLUMBING APPENDIX B - RATES OF RAINFALL - ALACHUA COUNTY, FL: 5 INCHES
 MAIN ROOF: FPC - PLUMBING TABLE 1106.2(2) - SIZE OF RECTANGULAR VERTICAL CONDUCTORS & LEADERS: 3" X 4" RAIN LEADER WILL PERMIT DRAINAGE OF 2,640 SQ. FT. OF ROOF AREA COVERAGE. WE WILL UTILIZE (3) 3" X 4" RAIN LEADERS EACH WITH EMERGENCY OVERFLOW SCUPPER SERVING (3) ZONE AREAS AS DESCRIBED BELOW:

ZONE 1: 542 SQ. FT.
 ZONE 2: 604 SQ. FT.
 ZONE 4: 316 SQ. FT.

ENTRANCE CANOPY: FPC - PLUMBING TABLE 1106.2.1 - SIZE OF CIRCULAR VERTICAL CONDUCTORS & LEADERS: 2" DIAMETER ROOF DRAIN WILL PERMIT DRAINAGE OF 575 SQ. FT. OF ROOF AREA. WE WILL UTILIZE (2) 2" DIAMETER ROOF DRAINS THAT DISCHARGE INTO (2) 3" X 3" DOWNSPOUTS AND SERVE THE ZONE AREAS DESCRIBED BELOW:

ZONE 5: 71 SQ. FT.
 ZONE 6: 71 SQ. FT.

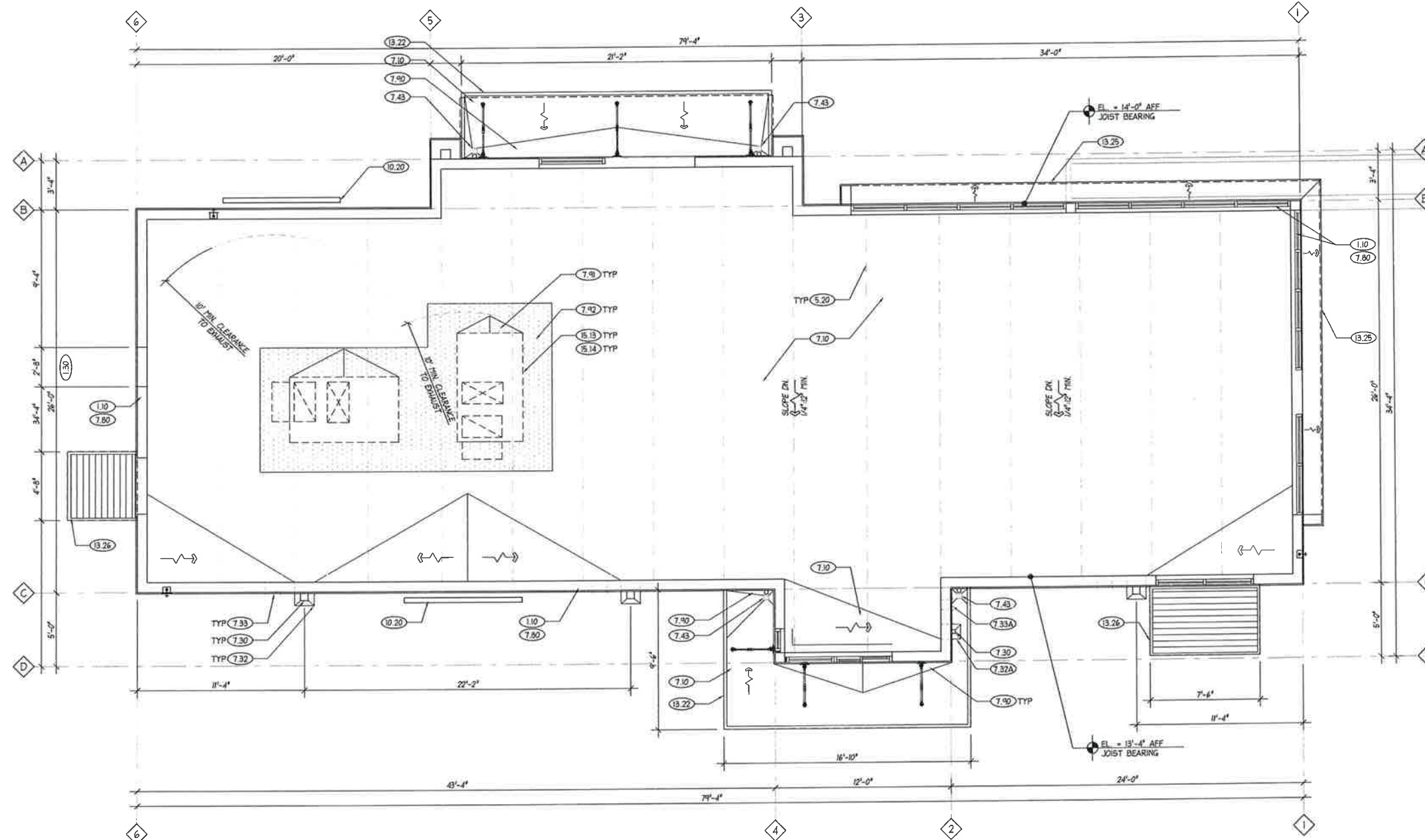
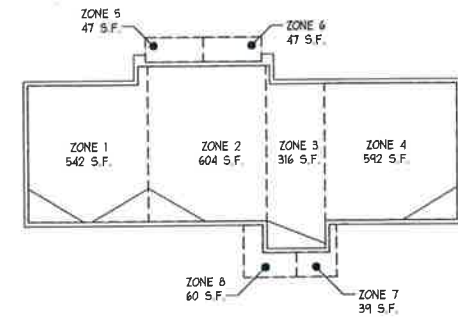
DRIVE-THRU ROOF: FPC - PLUMBING TABLE 1106.2(2) - SIZE OF RECTANGULAR VERTICAL CONDUCTORS & LEADERS: 3" X 4" RAIN LEADER WILL PERMIT DRAINAGE OF 2,640 SQ. FT. OF ROOF AREA COVERAGE. WE WILL UTILIZE (1) 3" X 4" RAIN LEADER WITH EMERGENCY OVERFLOW SCUPPER SERVING THE ZONE AREA AS DESCRIBED BELOW:

ZONE 3: 316 SQ. FT.

DRIVE-THRU CANOPY: FPC - PLUMBING TABLE 1106.2.1 - SIZE OF CIRCULAR VERTICAL CONDUCTORS & LEADERS: 2" DIAMETER ROOF DRAIN WILL PERMIT DRAINAGE OF 575 SQ. FT. OF ROOF AREA. WE WILL UTILIZE (2) 2" DIAMETER ROOF DRAINS THAT DISCHARGE INTO (2) 3" X 3" DOWNSPOUTS AND SERVE THE ZONE AREAS DESCRIBED BELOW:

ZONE 7: 39 SQ. FT.
 ZONE 8: 60 SQ. FT.

THE STATED VERTICAL CONDUCTORS AND ROOF DRAINS ARE ALL WITHIN ALLOWABLE LIMITS FOR AREA SERVED BASED ON RAINFALL RATES FOR THIS REGION



1 ROOF PLAN
 SCALE: 1/4" = 1'-0"
 NORTH

FWH
 Architects

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BOOS
 DEVELOPMENT GROUP

TO THE BEST OF THE KNOWLEDGE OF THE ARCHITECTS AND ENGINEERS, PLANS AND SPECIFICATIONS COMPLY WITH THE APPLICABLE MINIMUM BUILDING CODES AND THE APPLICABLE MINIMUM FIRE SAFETY STANDARDS

GUY F. FABER
 FL License No. AR0013323

revision descriptions
 date
 no.

STARBUCKS
 SHELL CONSTRUCTION
 5601 NW 43RD STREET
 GAINESVILLE, FL 32653

03.11.16
 date
 15076
 COMM. NO.
 ROOF PLAN

A1.1

**PRELIMINARY DRAWINGS
 NOT FOR CONSTRUCTION**

EXTERIOR FINISH SCHEDULE

WALL FINISH/ COLOR

- (EF-1) STUCCO: BENJAMIN MOORE: #HC-105 "ROCKPORT GRAY"
- (EF-2) STUCCO: BENJAMIN MOORE: #HC-166 "KENDALL CHARCOAL"
- (SD-1) 1"x6" WESTERN RED CEDAR, SHIPLAPPED CENTER MATCH A-CLEAR OR CLEAR HEART; BENJAMIN MOORE ARBORCOAT SEMI-SOLID STAIN "AMHERST GRAY"
- (BR-1) BRICK VENEER-BORAL BRICKS, "MANHATTAN"

METAL TRIM/COPING/ ETC.

- (MT-1) PAC-CLAD: "MATTE BLACK"
- (MT-2) KYNAR 500: "MATTE BLACK"
- (PT-1) BENJAMIN MOORE: #2120-10 "JET BLACK"
- (SF-1) STOREFRONT: "BLACK"

CONTRACTOR SHALL SUBMIT SAMPLES FOR STARBUCKS APPROVAL PRIOR TO ORDERING MATERIALS

FINISH NOTES

1. EIFS FINISH COAT COLORS SHALL BE MIXED TO MATCH PAINT COLORS INDICATED ON THE EXTERIOR FINISH SCHEDULE. PROVIDE 12" X 12" SAMPLES WITH SPECIFIED TEXTURE FOR REVIEW AND APPROVAL PRIOR TO ORDER AND APPLICATION
2. EIFS ACRYLIC FINISH COAT ON PORTLAND CEMENT STUCCO BASE SHALL HAVE MEDIUM DASH TEXTURE
3. ALL PLASTER REVEALS ARE TO BE PAINTED TO MATCH THE ADJACENT WALL COLOR UNLESS NOTED OTHERWISE
4. ALL ALUMINUM COPING SHALL BE PREFINISHED WITH FACTORY-APPLIED KYNAR 500 FINISH OR MANUFACTURER EQUIVALENT. REFER TO EXTERIOR FINISH SCHEDULE FOR COLOR
5. ALL PAINTABLE EXPOSED UTILITIES SHOULD BE PAINTED TO MATCH ADJACENT WALL SURFACE UNLESS OTHERWISE SPECIFIED.

ALL SIGNAGE TO BE PROVIDED BY OTHERS & MEET LOCAL CODE AS REQUIRED PER THE JURISDICTION

GLAZING CALCULATIONS

REQUIREMENTS: SUBAREA 1: 1ST FLOOR : 25% OF A.F.F (3'-8")

FACADE	FACADE AREA @ 3'-8" AFF	GLAZING AREA REQUIRED @ 3'-8" AFF	GLAZING PROVIDED @ 3'-8" AFF	TOTAL GLAZING PROVIDED
NORTH	176 SF	0 SF	6 SF	6 SF
EAST	409 SF	101 SF	196 SF	394 SF
SOUTH	176 SF	0 SF	103 SF	207 SF
WEST	397 SF	0 SF	60 SF	94 SF

NOTES:

FWH
Architects

LIC.# AA 003300

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GUY F. FABER
FL License No. AR0015323

revision descriptions

date

no.

STARBUCKS
SHELL CONSTRUCTION

5601 NW 43RD STREET
GAINESVILLE, FL 32653

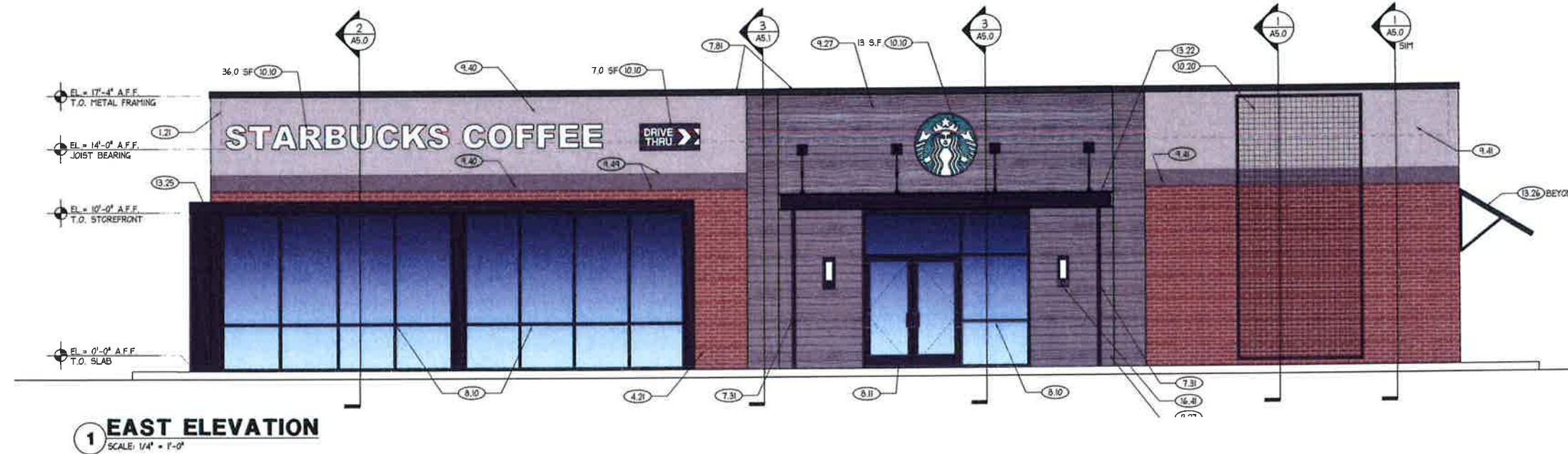
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COMM. NO.

EXTERIOR
ELEVATIONS

A3.0

PRELIMINARY DRAWINGS
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1 EAST ELEVATION
SCALE: 1/4" = 1'-0"



2 SOUTH ELEVATION
SCALE: 1/4" = 1'-0"

REFER TO SHEET A0.3 FOR KEYNOTE SCHEDULE

REFER TO SHEET A3.0 FOR EXTERIOR FINISH SCHEDULE AND GENERAL FINISH NOTES

ALL SIGNAGE TO BE PROVIDED BY OTHERS & MEET LOCAL CODE AS REQUIRED PER THE JURISDICTION

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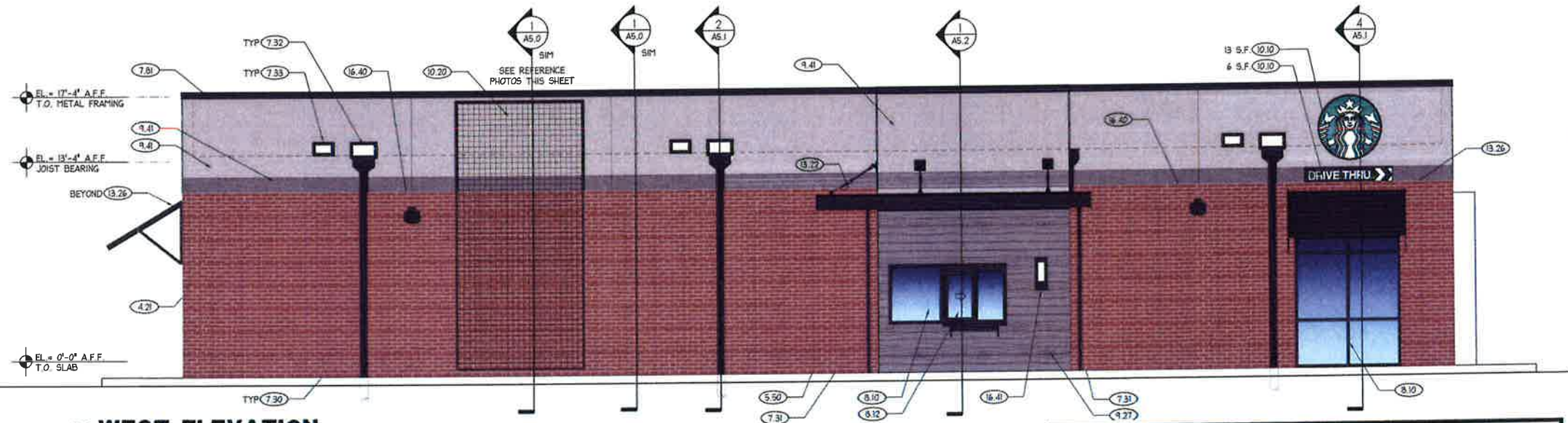
NO.	DATE	REVISION DESCRIPTIONS

STARBUCKS
SHELL CONSTRUCTION
5601 NW 43RD STREET
GAINESVILLE, FL 32653

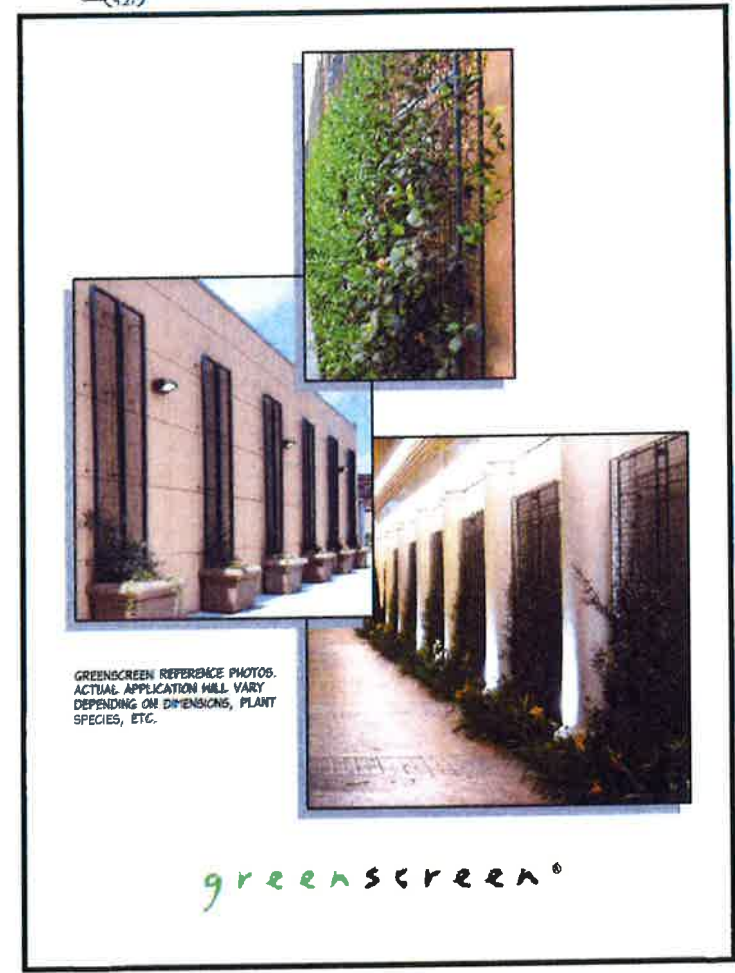
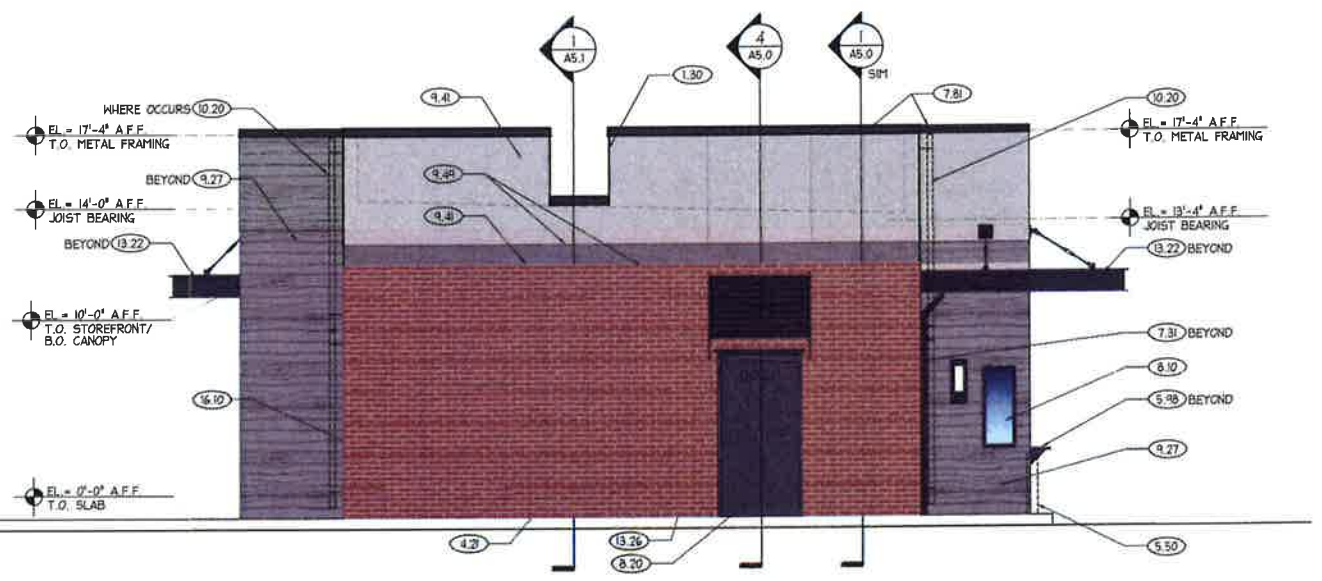
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EXTERIOR
ELEVATIONS

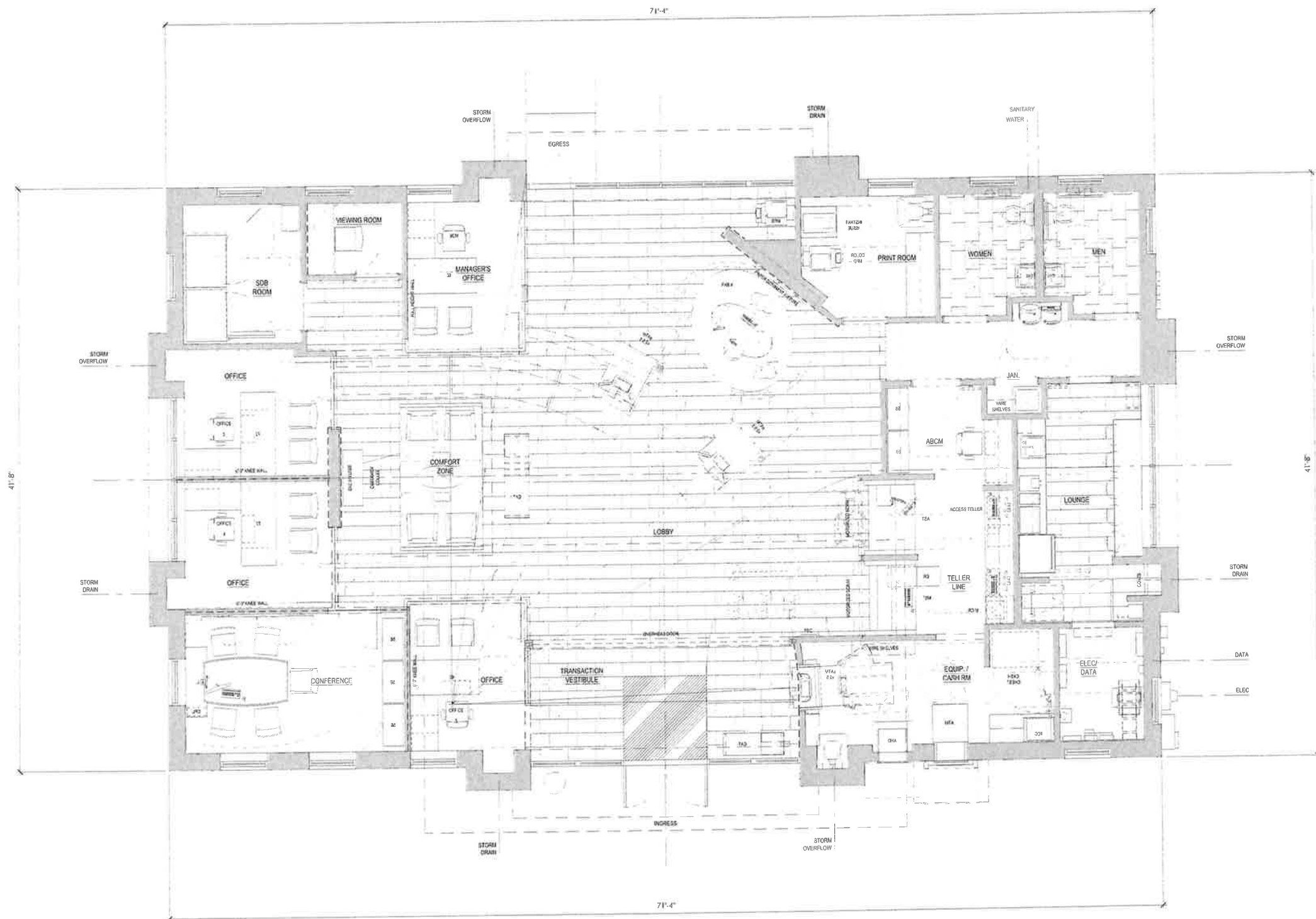
A3.1



3 WEST ELEVATION
SCALE 1/8" = 1'-0"



PRELIMINARY DRAWINGS
NOT FOR CONSTRUCTION



PROPOSED FLOOR PLAN

MARCH 30, 2016
 SCALE: 1/4" = 1'-0"

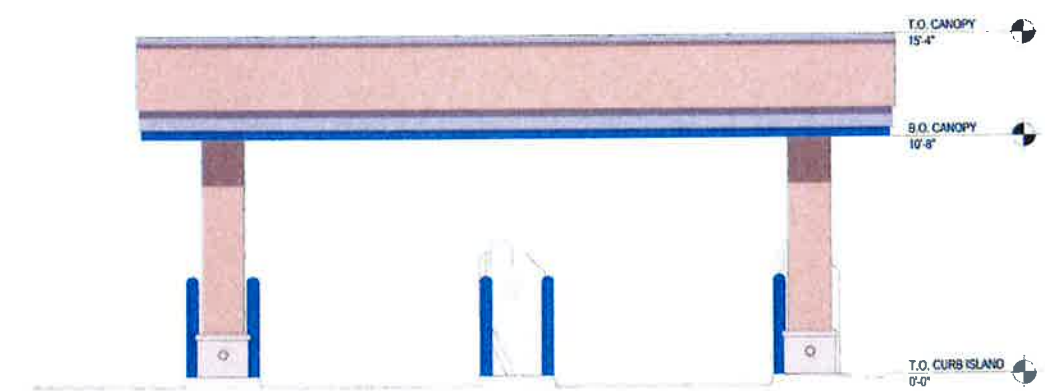


GLAZING CALCULATIONS:
 TOTAL SURFACE BETWEEN 3" AND 20" 328 SF
 REQUIRED GLAZING: 25% (82 SF)
 GLAZING PROVIDED:
 ENTRY @ 70 SF
 1 WINDOW @ 14 SF
 3 WINDOWS @ 2 SF
 TOTAL PROVIDED GLAZING: 90 SF

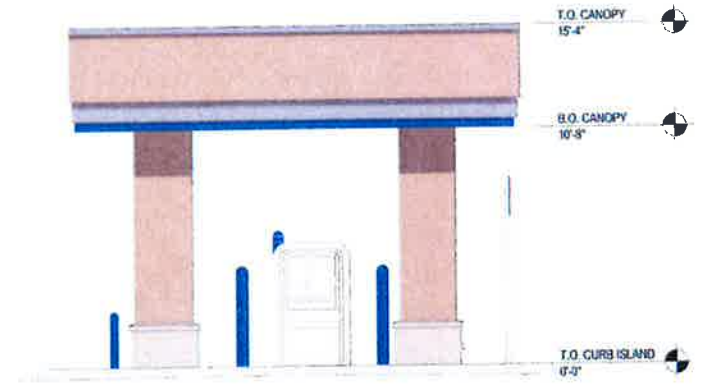
EAST ELEVATION



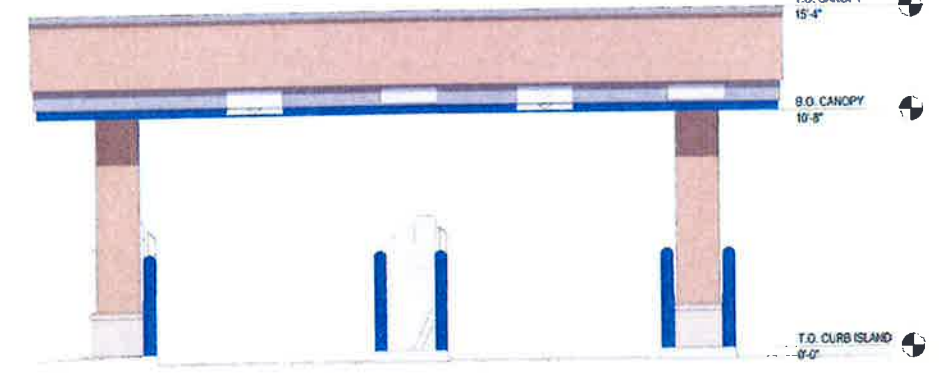
WEST ELEVATION



SOUTH ELEVATION



WEST ELEVATION



NORTH ELEVATION

PAINT - EP31
 APPLICATION: EXTERIOR MATERIAL
 COLOR: MATCH TO SHERWIN WILLIAMS
 SW-7845 'INTELLECTUAL GRAY'

PAINT - EP22
 APPLICATION: EPS
 COLOR: TO MATCH SHERWIN WILLIAMS
 SW-7036 'ACCESSIBLE BEIGE'

PAINT - EP14
 APPLICATION: EPS
 COLOR: TO MATCH SHERWIN WILLIAMS
 SW-6108 'LATE'

ROOF
 APPLICATION: BERRIDGE METAL ROOF
 COLOR: ZINC GREY

MILLIONS
 APPLICATION: EXTERIOR MATERIAL
 COLOR: ANNOXIDIZED ALUMINUM

STONE - ST1
 APPLICATION: SAVANNAH STONE
 COLOR: LIMESTONE #3

METAL
 APPLICATION: CANOPY/TRIM
 COLOR: BLUE

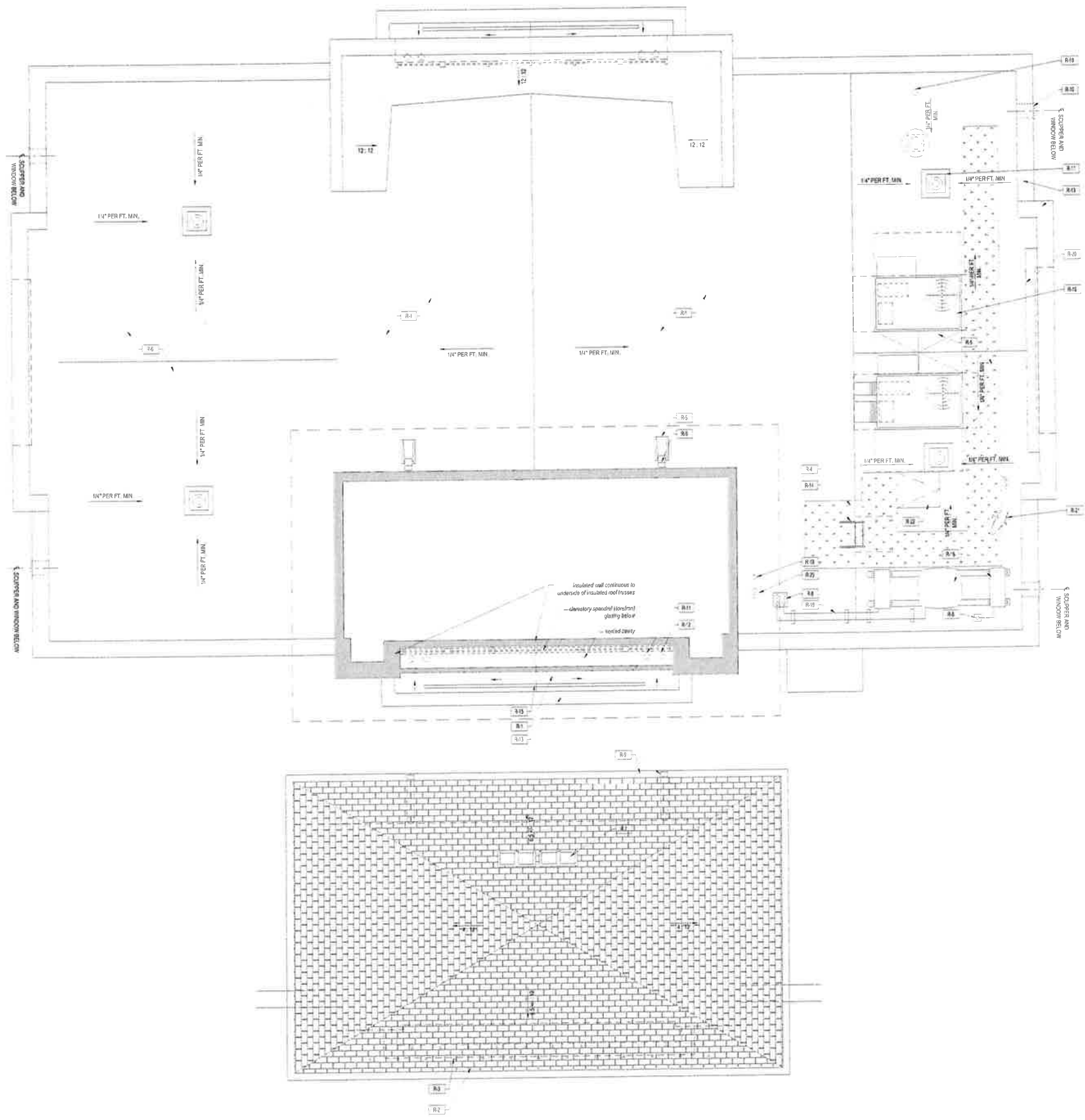
CHASE
 132 NW 76TH DRIVE
 GAINESVILLE, FLORIDA

PROPOSED EXTERIOR ELEVATIONS

MARCH 30, 2016
 SCALE: 3/16" = 1'-0"

110 N. 11TH ST. SUITE 101
 TAMPA, FL 33619
 Contact: Paul Roberts
 proberts@core-eng.com

CORE STATES
 GROUP



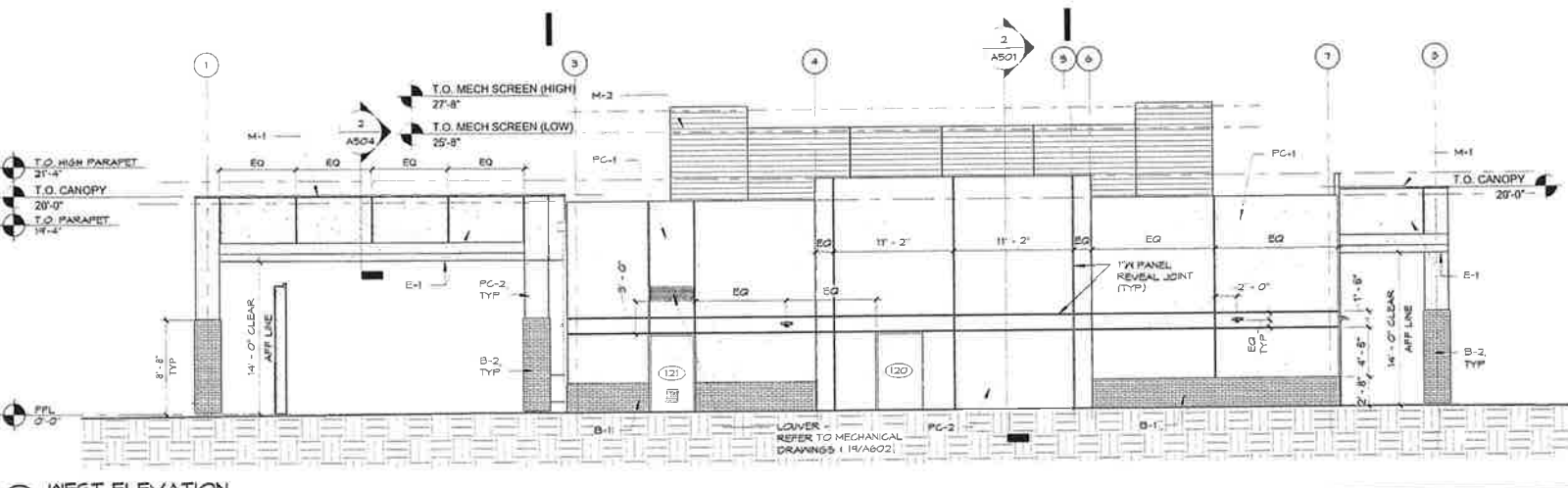
ROOF PLAN NOTES	
R-1	LOW-SLOPE ROOFING: REFER TO WALL SECTIONS AND CONSTRUCTION TYPES
R-2	SHINGLE ROOFING: REFER TO WALL SECTIONS AND CONSTRUCTION TYPES
R-3	SNOW GUARDS: SHOP-FABRICATED PRE-FINISHED METAL CLIPS COMPATIBLE WITH ROOFING AND SUBSTRATE. COLOR-MATCH TO FINISH ROOFING. REQUIRED ONLY AT ROOF EDGES OVER PEDESTRIAN WALKS IN REGIONS WITH SUBSTANTIAL SNOWFALL.
R-4	ROOFING TRAFFIC PADS: SURFACE-ADHERED TEXTURED WALKWAY MATERIAL PER ROOFING MANUFACTURER SPECIFICATIONS TO ALL MECHANICAL EQUIPMENT
R-5	SPLASH BLOCK: PRE-FABRICATED CONCRETE BLOCK WITH DRAINAGE CHANNEL SET ON MIN 60 MIL ROOFING MATERIAL PAD OR SUILT UP 60 MIL ROOFING LAYERS AS REQD. TO PREVENT PREMATURE WEARING OF THE PRIMARY ROOFING MEMBRANE
R-6	FLAT ROOF DRAINAGE PLANES: INSTALL TAPERED INSULATION AS REQUIRED TO FORM RIDGES, VALLEYS, CRICKETS AND SADDLES AS REQUIRED TO ACHIEVE MINIMUM 1/4" PER FOOT PITCH OR MANUFACTURER'S MINIMUM PITCH REQUIREMENTS FOR THE APPLICATION, WHICHEVER IS GREATER. FORM SLOPES TO ELIMINATE THE POSSIBILITY OF PONDING
R-7	ROOF VENTS: SHOP-FABRICATED PRE-FINISHED CORROSION-RESISTANT METAL CONVECTIVE VENTS WITH INSECT SCREENS. METAL SHALL BE COMPATIBLE WITH ROOFING AND SUBSTRATE. COLOR-MATCH TO FINISH ROOFING
R-8	PIPE PENETRATION: PRE-FABRICATED WATER-TIGHT SURFACE-ADHERED ROOFING PORTAL COMPATIBLE WITH ROOFING MATERIAL
R-9	GUTTER AND DOWNSPOUT SYSTEM: • SHOP-FABRICATED PRE-FINISHED ALUMINUM • REFER TO EXTERIOR ELEVATIONS FOR COLOR • 6" K-STYLE GUTTER ON CONCEALED ALUMINUM CLIP HANGERS AT 24" o.c. MAX. • CORRUGATED RECTANGULAR DOWNSPOUTS • SIZED AS REQUIRED- WITH LEAF BASKETS • SECURE DOWNSPOUTS TO FACADE WITH PRE-FAB. ALUM. DOWNSPOUT BRACKETS AT 48" o.c. MAX.
R-10	SCUPPER: WATER-TIGHT SHEET METAL THROUGH-WALL DRAINAGE CHANNEL. REFER TO DETAILS AS NOTED.
R-11	ROOF DRAIN: METAL DRAIN PAN AND BASKET ASSEMBLY WITH CLAMP RING SECURED WATER-TIGHT TO ROOFING MEMBRANE. REFER TO PLUMBING FIXTURE SCHEDULE
R-12	OVERFLOW DRAIN: METAL DRAIN PAN AND BASKET ASSEMBLY WITH CLAMP RING SECURED WATER-TIGHT TO ROOFING MEMBRANE. REFER TO PLUMBING FIXTURE SCHEDULE
R-13	PARAPET COPING SYSTEM: SHOP-FABRICATED PRE-FINISHED ALUMINUM. REFER TO WALL SECTIONS FOR DETAILS AND EXTERIOR ELEVATIONS FOR COLOR
R-14	ROOF LADDER: REFER TO DETAILS AS NOTED
R-15	LIGHT FIXTURE: SURFACE-MOUNT ELECTRICAL UPLIGHT FIXTURE SECURED TO BLOCKING OVER ROOFING MEMBRANE. REFER TO LIGHT FIXTURE SCHEDULE AND WALL SECTIONS
R-16	MECHANICAL EQUIPMENT: VENTILATION UNIT, PACKAGED ROOFTOP HEATING AND COOLING UNIT OR SPLIT-SYSTEM AC UNIT MOUNTED TO SHOP-FAB. VIBRATION-ISOLATING RAIL OR INSULATED CURB UNIT. LAP FLASH WATER-TIGHT TO ROOFING MEMBRANE. RUN CONDENSATE LINES TO ROOF DRAINS TO STORM
R-17	FLAT SEAM METAL ROOFING
R-18	PLUMBING STACK VENT: PVC SIZED AS REQD. WITH PRE-FABRICATED COMPATIBLE FLASHING BOOT ADHERED TO ROOF MEMBRANE. EXTEND VENT TO ALIGN WITH ADJACENT PARAPET COPING AND PROVIDE LATERAL BRACING AS REQD.
R-19	SURFACE-MOUNT MECHANICAL PIPING: GAS AND REFRIGERANT PIPING SUPPORTED ON ROOFING BY B-LINE DURA-BLOK PRE-FAB RUBBER-BASE GALVANIZED STEEL UNISTRUT PIPE SUPPORTS AT 8'-0" o.c. MAX.
R-20	SURFACE-MOUNT SIGNAGE EQUIPMENT ENCLOSURE: WATER-TIGHT SHOP-FABRICATED METAL SIGNAGE ENCLOSURE BY SIGN VENDOR. SECURE TO BACKSIDE OF PARAPET WALL WITH MINIMAL PENETRATIONS OF ROOFING MEMBRANE. PROVIDE PERIMETER WATER-TIGHT SEALANT COMPATIBLE WITH ROOF MEMBRANE
R-21	SATELLITE ANTENNA: ANTENNA ASSEMBLY BY OWNER'S EQUIPMENT VENDOR. PROVIDE WATER-TIGHT CAPPED HOT-DIPPED GALVANIZED STEEL PIPE STANCHION SIZED AS REQUIRED. FLASHED WATER-TIGHT TO ROOF WITH PRE-FAB COMPATIBLE FLASHING BOOT ADHERED TO ROOF MEMBRANE
R-22	SCUTTLE: PRE-FAB. INSULATED ROOF SCUTTLE WITH RETRACTABLE SAFETY POST. FLASH WATER-TIGHT TO ROOFING MEMBRANE. PROVIDE CARABINER THROUGH LATCH
R-23	FURNACE VENT: CONCENTRIC OR SIDE-WALL PVC EXHAUST/ COMBUSTION AIR INTAKE W/ ROOF MEMBRANE FLASHING BOOT. AT GAS-FIRED FURNACE LOCATIONS ONLY



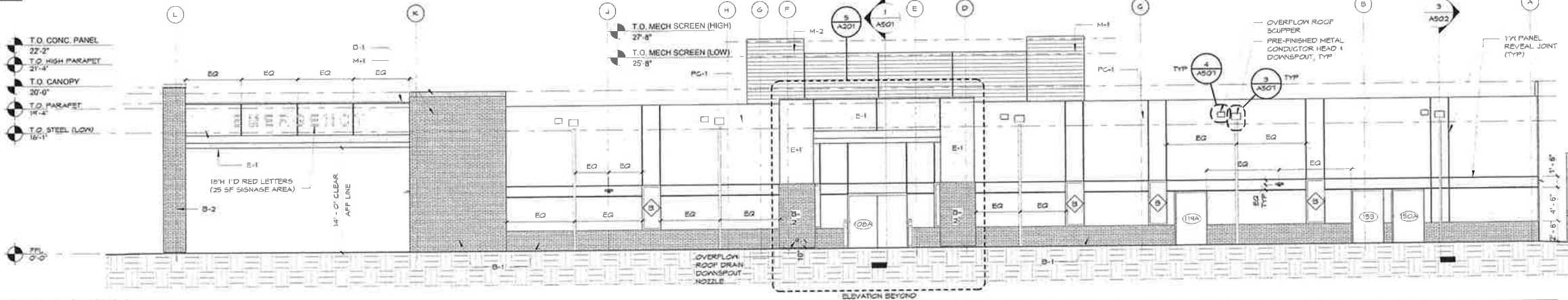
HEREFORD · DOOLEY
ARCHITECTS
1720 WEST END AVE · SUITE 300
NASHVILLE · TENNESSEE · 37203
P · 615 · 244 · 7399
F · 615 · 244 · 6697
WWW.HDARCHITECTS.COM

EXTERIOR FINISH SCHEDULE			
MARKER	PRODUCT	MANUFACTURER	COLOR/FINISH
PC-1	INTEGRAL COLORED PRECAST INSULATED CONCRETE PANEL (FIELD COLOR)	REFER TO SPECIFICATIONS	SANDBLAST FINISH, INTEGRAL COLOR TO MATCH SW6127 (IVOIRE)
PC-2	INTEGRAL COLORED PRECAST INSULATED CONCRETE PANEL (ACCENT COLOR)	REFER TO SPECIFICATIONS	SANDBLAST FINISH, INTEGRAL COLOR TO MATCH SW6100 (PRACTICAL BEIGE)
B-1	THIN BRICK VENEER (SOLDIER COURSE & RUNNING BOND)	CAROLINA CERAMICS	DESERT TAN VELOUR
B-2	THIN BRICK VENEER (RUNNING BOND)	CAROLINA CERAMICS	PEBBLE BEACH VELOUR
B-3	MODULAR FACE BRICK VENEER (RUNNING BOND)	CAROLINA CERAMICS	DESERT TAN VELOUR
E-1	EIFS (FASCIA) & ACRYLIC PLASTER FINISH (UNDERSIDE OF CANOPY)	STO SYSTEMS OR DRYVIT SYSTEMS, INC	SANDBLEBBLE FINE TEXTURE, COLOR TO MATCH SW6100 (PRACTICAL BEIGE)
M-1	PRE-FINISHED METAL COPING	REFER TO SPECIFICATIONS	COLOR TO MATCH SW6127 (IVOIRE)
M-2	CORRUGATED ROOF SCREEN WALL PANELS, COPING, EDGE & CORNER TRIM	REFER TO SPECIFICATIONS	COLOR TO MATCH SW6127 (IVOIRE)
X	ALUMINUM WINDOW FRAMES	REFER TO SPECIFICATIONS	CLEAR ANODIZED
X	GLASS/GLAZING-VISION	REFER TO SPECIFICATIONS	SUNGUARD LIGHT BLUE 63
***	EXTERIOR HM DOORS, FRAMES & LOUVERS	REFER TO SPECIFICATIONS	PAINT TO MATCH THE PRECAST PANEL THE COMPONENT IS LOCATED IN

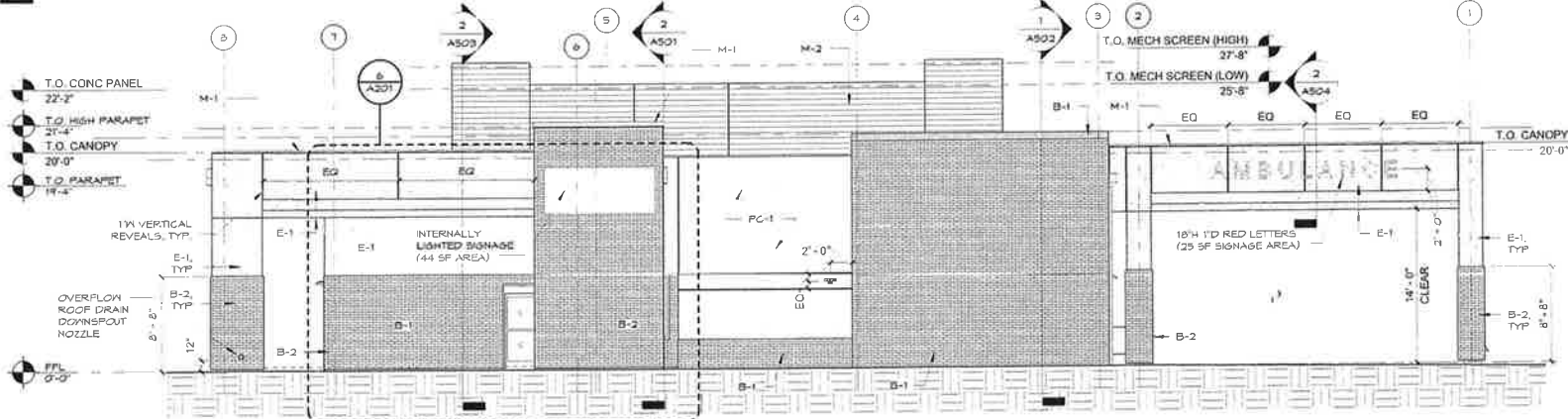
THIS DRAWING IS THE PROPERTY OF HEREFORD DOOLEY ARCHITECTS. IT IS NOT TO BE REPRODUCED IN WHOLE OR IN PART. IT IS NOT TO BE USED ON ANY OTHER PROJECT. IT SHALL BE RETURNED UPON REQUEST. COPYRIGHT © HEREFORD DOOLEY ARCHITECTS. NOT VALID UNLESS SIGNED AND SEALED.
 C:\Users\Sara.Fredrick\Documents\215044_00_Colts FSER_CENTRAL14_Sara.Fredrick.rvt
 10/27/2015 9:02:09 AM



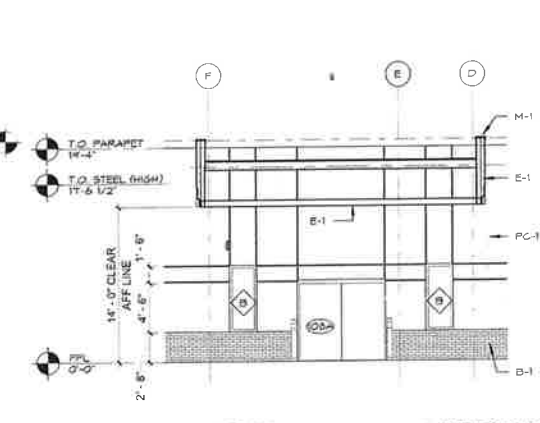
1 WEST ELEVATION
1/8" = 1'-0"



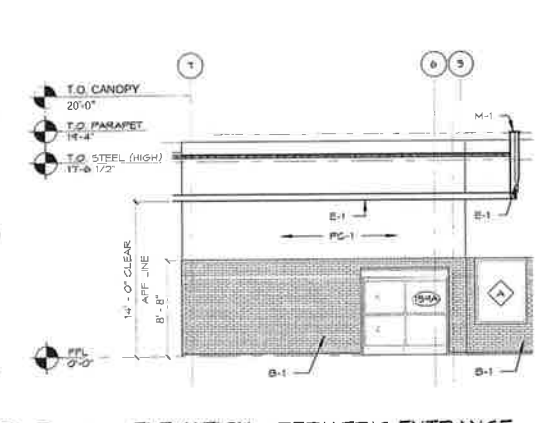
2 NORTH ELEVATION
1/8" = 1'-0"



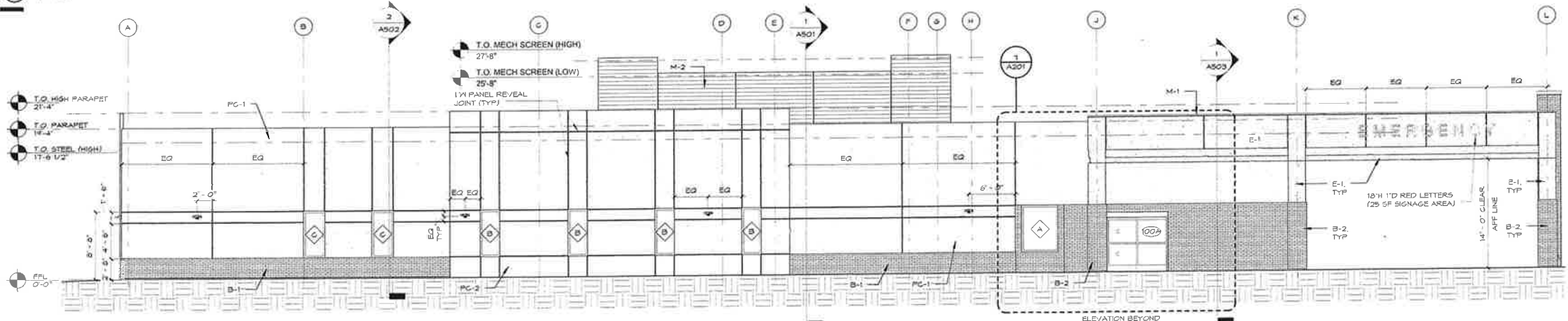
3 EAST ELEVATION
1/8" = 1'-0"



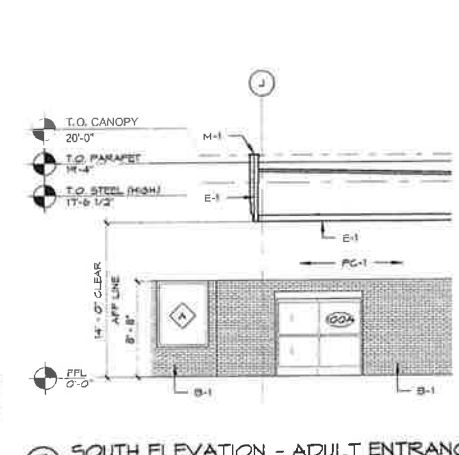
5 NORTH ELEVATION - AMBULANCE ENTRANCE
1/8" = 1'-0"



6 EAST ELEVATION - PEDIATRIC ENTRANCE
1/8" = 1'-0"



4 SOUTH ELEVATION
1/8" = 1'-0"



7 SOUTH ELEVATION - ADULT ENTRANCE
1/8" = 1'-0"

PROJECT _____
LOCATION _____
CLIENT _____

CONSTRUCTION DOCUMENTS

11-02-2015

DATES OF ISSUANCE

EXTERIOR ELEVATIONS

A201

SHEET

Attachment “C”

Application and Neighborhood Workshop Information



April 5, 2016

City of Gainesville
Planning Division
P.O. Box 490
Gainesville, Florida 32602

Ref: NW 43rd Street/NW 53rd Ave Retail Development
Performance Standards Letter

Dear Staff:

Pursuant to Section 30-345 of the City's LDC we are providing this letter as satisfaction of the General Performance Standards as set forth in the section.

The NW 43rd Street/NW 53rd Ave Retail Development project meets the standards as follows:

- (1) *Fire and explosion hazards*: Limited storage of flammable or explosive materials is proposed at the site.
- (2) *Radiation*: No radiation operations are proposed for this development.
- (3) *Electromagnetic radiation*: No electromagnetic radiation is proposed for the site.
- (4) *Smoke and other particulate matter*: No smoke or particulate matter will be discharged from this development without proper treatment and permitting with ACEPD.
- (5) *Toxic or noxious matter*: No activity involving toxic or noxious matter is proposed for this development.
- (6) *Waste disposal*: This site will not discharge waste (liquid or solid) that will result in a violation of local, state or federal regulations.
- (7) *Vibration*: This use will not create any vibrations as defined in the section.
- (8) *Heat, cold, dampness, or movement of air*: The proposed use of the site will not affect temperature, motion or humidity of the atmosphere.
- (9) *Glare*: The site plan meets the City's lighting ordinance and therefore meets the performance standard for glare.
- (10) *Noise, Odor, etc*: Use of the site will not create noise, odor or any other irritant that will unreasonably disturb the adjoining lands.

In addition, all proposed utilities are to be underground except for those apertures (ie: transformers, fire hydrants, etc) that are defined by code to be allowed to be above grade.

Sincerely,

CHW

Holly Simon, EI

Project Manager

L:\2015\15-0490\Engineering\City County\160405 Submittal\LTR 160405 NW 43rd 53rd Dev CofG Performance Standards Letter.docx

GAINESVILLE APPLICATION FOR ENVIRONMENTAL REVIEW

OFFICE USE ONLY	
Petition No. _____	Fee: \$ _____
Tax Map No. _____	Receipt No. _____
Account No. 001-660-6680-4063	

CHECK ONE:

Basic Level 1 Level 2 Submittal: 1st 2nd 3rd

Basic Environmental Review – Submit general environmental assessment with application.
Level 1 Environmental Review – Submit environmental studies with application.
Level 2 Environmental Review – Submit mitigation and/or management plan.
 (Review fees are set in accordance with the most current fee schedule. The environmental review fee includes a maximum of three reviews within 2 years per project.)

Owner(s) of Record (please print)	Applicant(s)/Agent(s) (please print)
Name: Jeff Montgomery Associates, LLC	Name: Boos Development Group/CHW
Address: 2357 NW 13th Place Gainesville, Fl 32605	Address: 132 NW 76th Drive Gainesville, Fl 32607
E-mail: Please contact agent	E-mail: hollys@chw-inc.com
Phone: _____ Fax: _____	Phone: 352 331-1976 Fax: 352 331-2476
<i>(If additional owners, please include on back)</i>	

PROJECT INFORMATION

Project Name	NW 43rd/53rd Retail Development
Check all regulated resources that apply to this development application:	
Regulated Surface Waters & Wetlands (LDC 30-300)	<input type="checkbox"/> Surface Waters and/or Wetlands
Regulated Parks & Conservation Areas (LDC 30-307)	<input type="checkbox"/> Nature Park and Public Conservation/Preservation Areas District
Regulated Natural & Archaeological Resources (LDC 30-310)	<input type="checkbox"/> Floridan Aquifer High Recharge Area
	<input type="checkbox"/> Significant Natural Communities
	<input type="checkbox"/> Listed Species
	<input type="checkbox"/> Strategic Ecosystems
	<input type="checkbox"/> Significant Archaeological Resources
	<input type="checkbox"/> Significant Geological Resource Features

I certify that the above statements are correct and true to the best of my knowledge.


 Applicant's signature

4/4/10
 Date

Certified Cashier's Receipt:

Current Planning
 Planning Counter—158

Phone: 352-334-5023

Thomas Center B
 306 NE 6th Avenue

**DEVELOPMENT PLAN REVIEW APPLICATION
PLANNING & DEVELOPMENT SERVICES**

OFFICE USE ONLY	
Petition No. _____	Fee: \$ _____
1 st Step Mtg Date: _____	EZ Fee: \$ _____
Account No. 001-660-6680-3401 []	
Account No. 001-660-6680-1124 (Enterprise Zone) []	
Account No. 001-660-6680-1125 (Enterprise Zone Credit) []	

LEVEL OF REVIEW (check one)

MINOR	INTERMEDIATE	MAJOR	CONCEPT	MASTER
		X		

Project Name: NW 43rd/53rd Retail Development

Property Address: north west corner of NW 43rd St and NW 53rd Ave intersection

Tax Parcel #(s): 06023-001-000

Site Area (acres): 2.57

CHECK ALL PROPOSED USES

<input type="checkbox"/> Residential	Density	<input checked="" type="checkbox"/> Non-residential
Multi-family	Units/acre:	<input checked="" type="checkbox"/> Commercial <input type="checkbox"/> Office
Total units:	Total bedrooms:	<input type="checkbox"/> Industrial <input type="checkbox"/> Other
		Gross floor area: 16,120

Owner(s) of Record (please print)
Name: <u>Jeff Montgomery Associates, LLC</u>
Address: <u>2357 NW 13th Place</u>
<u>Gainesville, Fl 32605</u>
Phone: <u>contact agent</u> Fax: _____
(If additional owners, attach information)

Applicant(s)/Agent(s), if different
Name: <u>Boos Development Group/CHW</u>
Address: <u>132 NW 76th Drive</u>
<u>Gainesville, Fl 32607</u>
Phone: <u>352 331-1976</u> Fax: <u>352 331-2476</u>
(If additional agents, attach information)

I certify that I am the owner of the property and authorize the agent listed above to initiate this development plan.

Signature of owner: _____ Date: _____

I certify that all of the information contained in this application form is accurate and up-to-date.

Signature of Applicant: [Signature] Date: 4/4/16

STATE OF FLORIDA, COUNTY OF Alachua
 Sworn to and subscribed before me this 4th day of April 2016
 Signature – Notary Public: Wendy L Mercer
 Personally Known OR Produced Identification _____

Certified Cashier's Receipt:





APPLICATION FOR SPECIAL USE PERMIT
Planning & Development Services

OFFICE USE ONLY	
Petition No. _____	Fee: \$ _____
1 st Step Mtg Date: _____	EZ Fee: \$ _____
Tax Map No. _____	Receipt No. _____
Account No. 001-660-6680-3401 []	
Account No. 001-660-6680-1124 (Enterprise Zone) []	
Account No. 001-660-6680-1125 (Enterprise Zone Credit) []	

Application for a special use permit will be accepted for review only after a pre-application conference (First Step Meeting). Application to be completed by applicant. Application must include a preliminary development plan. Incomplete applications will be returned to the applicant.

Name of Owner(s) (please print)
Name: Jeff Montgomery Associates, LLC
Address: 2357 NW 13th Place Gainesville, Fl 32605
Phone: Please contact Agent
Owner's Signature: _____
(If additional owners, please include on back)

Applicant(s)/Agent(s), if different
Name: Boos Development Group/CHW
Address: 132 NW 76th Drive Gainesville, Fl 32607
Phone: 352 331-1976 Fax: 352 331-2476

PROPERTY INFORMATION: (Information below applies to property for which a Special Use Permit is being requested.)
Street address: north west corner of NW 43rd St and NW 53rd Ave intersection
Tax parcel no(s): 06023-001-000
Legal description (use separate sheet, if needed): See attached

I hereby attest to the fact that the above supplied parcel number(s) and legal description(s) is (are) the true and proper identification of the area for which the permit is being requested.

Signature of applicant: Hal S. Date: 4/4/16

Certified Cashier's Receipt:

A Special Use Permit is requested pursuant to Section 30, Subsection 232&233, Paragraph _____, of the Land Development Code, City of Gainesville, to allow the following use:
Drive thru facilities to serve retail and medical facilities

A preliminary site plan is/is not required and is/is not attached.

Existing zoning classification: MU-1 Existing land use designation: MUL

Existing use of property: commercial, vacant building

SURROUNDING PROPERTY INFORMATION: (List all uses surrounding the subject property under "Existing use." Staff is available to supply zoning and land use information.)			
	Zoning	Land Use	Existing Use
North	OF	OF	Offices
South	PD	MUL	Commercial/retail
East	PS	PF	Public Services
West	RSF-1	SF	Single Family Residential

TO THE APPLICANT: (Please sign the bottom of this application after you have read the following.)

- The City of Gainesville will notify owners of property within 400 feet of the subject property of this application.
- No application for a Special Use Permit shall be entertained within 2 years after the denial or withdrawal of a request for the same use for the same property.
- The City Plan Board's decision concerning a Special Use Permit may be appealed by the applicant to a hearing officer within 15 days of the date notification of the decision is sent by certified mail to the applicant.

Signature: *Hal S.* Date: 4/4/16

Name of Owner (please print)
Name: Jeff Montgomery Associates, LLC
Address: 2357 NW 13th Place Gainesville, FL 32605
Phone: contact agent Fax:
Owner's Signature:
(If additional owners, please list on separate sheet)

Name of Owner (please print)
Name:
Address:
Phone: Fax:
Owner's Signature:

Reference: Chapter 30, Land Development Code
 City Code of Ordinances, Article VII, Division 5



**APPLICATION FOR CONCURRENCY CERTIFICATION & TMPA REVIEW
DEPARTMENT OF PLANNING & DEVELOPMENT SERVICES**

**LONG FORM
(352) 334-5022**

OFFICE USE ONLY	
Petition No. _____	TMPA Zone [] A [] B [] C [] D [] E [] M

<u>TYPE OF CERTIFICATION REQUESTED:</u>
[] Concurrency Determination (non-binding)
[] Certificate of Preliminary Concurrency
[X] Certificate of Final Concurrency
[] Certificate of Conditional Concurrency Reservation

Owner Name(s) (please print)
Name(s): Jeff Montgomery Associates, LLC 2357 NW 13th Place Gainesville, Fl 32605
E-Mail Address:
Phone: contact agent Fax:
(If additional owners, please include on separate sheet)

Agent(s) Name (please print)
Name: Boos Development Group/CHW Mailing Address: 132 NW 76th Drive Gainesville, Fl 32607
E-Mail Address: hollys@chw-inc.com
Phone: 352 331-1976 Fax: 352 331-2476
(Attach notarized authorization for agent to act on owner's behalf.)

PROJECT INFORMATION	
Project Name: NW 43rd/53rd Retail Development	Phase: 1
Location of Project (attach an 8 1/2" x 11" map showing location)	
1. Street address: south of Windmeadows Blvd, east of SW 35th Blvd, north of Archer Road, and west of SW 34th St	
2. Legal description (may be attached): See attached	
3. Tax parcel number(s): 06023-001-000	4. Map number(s):
Existing Land Use Category: MUL	Existing Zoning: MU-1

Is there a proposal to change the zoning and/or land use associated with this project? [] Yes [X] No
If yes, indicate petition number(s) associated with change:

PHASING			
Is this project (phase) part of a larger project?	[] Yes	[X] No	
If yes, enumerate each phase, number of units or square footage in each phase and beginning/ completion date.			
Total Project: Residential units	SF	SFA	MF
Non-residential (square footage) 16,120			
Mixed-use (describe mix)			
(If this is a single phase project, name it Phase I – Total)			

RESIDENTIAL DATA					
Type	Phase	Number of units	Acres	Expected beginning date	Expected completion date
Single-family, detached					
Single-family, attached					
Multi-family					
Rooming houses or dormitories (beds)					
Other (specify)					

NON-RESIDENTIAL DATA					
Type(s) specify	Phase	Square footage	Acres	Expected beginning date	Expected completion date
Retail, Medical	1	16,120	2.5	July 2016	July 2017

STOP HERE AND SIGN CERTIFICATION ON PAGE 3 IF YOU ARE REQUESTING ONLY A CONCURRENCY DETERMINATION

Required Information for Certificates of Preliminary, Final, and Conditional Concurrency Reservation & TMPA Review (Attach sheets to application.)

1. Attach a sheet with the average daily, and peak hour, peak direction trip generation for the project based on the latest edition of the ITE Trip Generation Manual. (**NOTE: The trip generation information MUST be attached to this application and shown on the development plan.**) In cases where the City and the applicant show differences in projected trips, the applicant’s calculations must be signed and sealed by the professional engineer registered in the State of Florida.

Application for Concurrency Certification & TMPA Review

- 2. Is the proposed project within the Transportation Mobility Program Area (TMPA) (see attached map)? If yes, please be aware that special criteria apply in this area. [] Yes [] No
Zone A [] Zone B [x] Zone C [] Zone D [] Zone E [] Zone M []
- 3. Indicate whether the proposed project will be eliminating any existing recreation facilities. If yes, detail the number and type being eliminated. [] Yes [x] No
- 4. Submit a complete stormwater management plan for water quantity and water quality review by the City's Public Works Department. (Do not submit with this application, submit with the development plan.)
- 5. Does this application involve demolition or re-use of any structure(s)? [] Yes [x] No

If yes, what is the size of the structure(s) to be demolished or re-used? N/A (unit(s) or square footage)

What is the current use of the structure to be demolished or re-used?

Are you claiming trip credits for the demolition or re-use of a structure(s) at the site?
[x] Yes [] No

If yes, provide estimates of credits for each previous use at the site. (Attach sheet with calculations.)

□ □ □ □ □

Certification

The undersigned has read the above application and is familiar with the information submitted herewith. It is agreed and understood that the undersigned will be held responsible for its accuracy. The undersigned hereby attests to the fact that the parcel number(s) and legal description(s) shown in questions 2 and 3 is/are the true and proper identification of the area for which the concurrency application is being submitted. Signatures of all owners or their agent are required on this form.

NOTE: The undersigned agrees that signing this application grants Planning staff the right to amend, for the sake of consistency, the square footage or number of units shown herein based on changes made to the development plan, subdivision, special use permit, or planned development during the review process.

[Signature]
Owner/Agent Signature

4/4/16
Date

STATE OF FLORIDA
COUNTY OF Alachua
Sworn to and subscribed before me this 4th day of April 2016.

Wendy L Mercer
Signature - Notary Public

Personally Known OR Produced Identification



TRANSPORTATION MOBILITY ELEMENT

Transportation Mobility
Program Area

Legend

Transportation Mobility
Program Area Sub-Zones

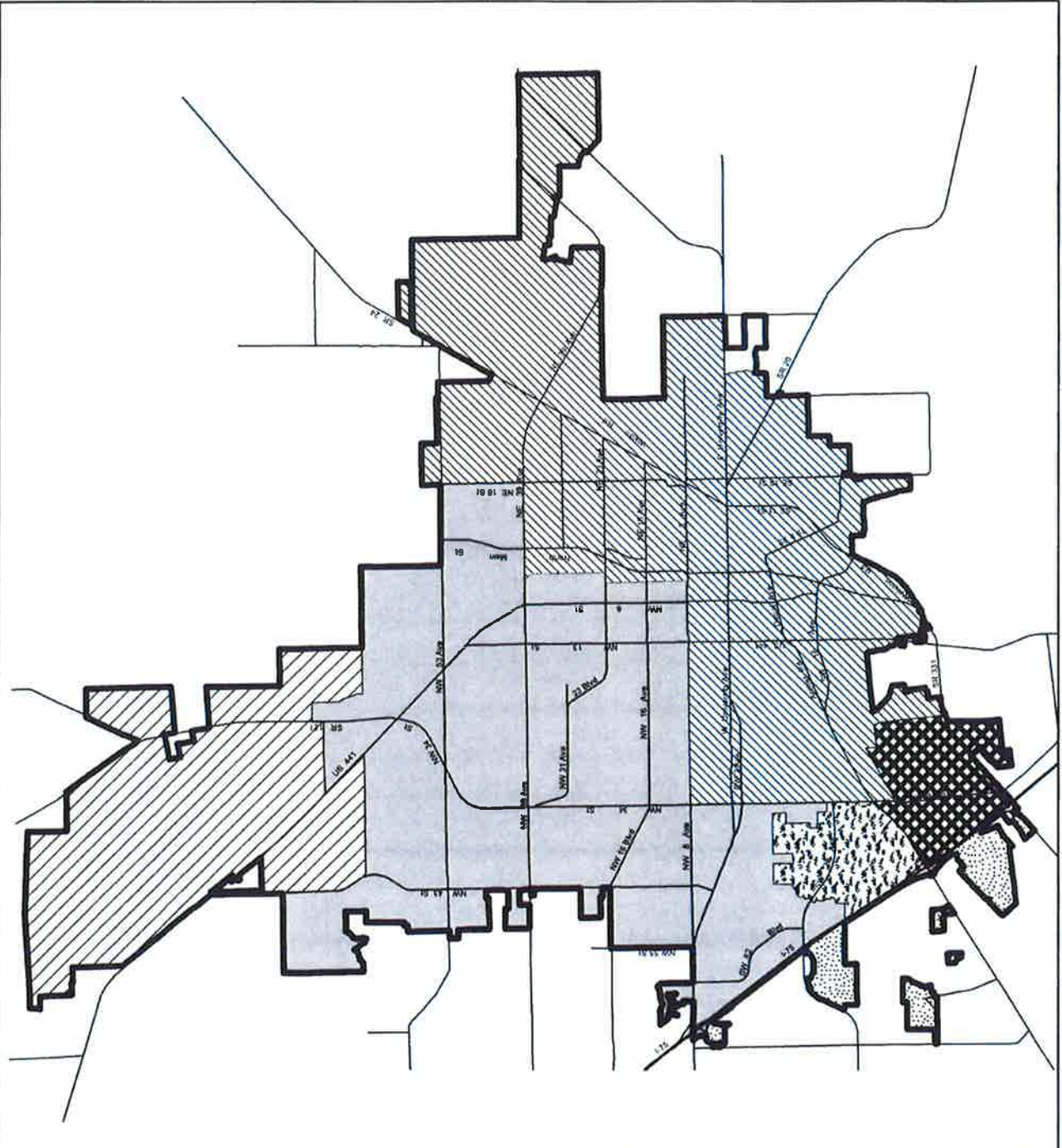
- Zone A
- Zone B
- Zone C
- Zone D
- Zone E
- Zone M

Gainesville City Limits

**City of Gainesville
Gainesville, Florida**

Prepared by Planning and
Development Services
November 2012

0 1 2 Miles



STAFF USE ONLY

Estimated demand:

Potable water (_____ units x 2.25) x 200 = _____ peak gallons per day (resid. only)

Water Supply _____ (see GRU)

Wastewater (_____ units x 2.25) x 113 = _____ average gallons per day (resid. only)

Solid Waste (_____ units x 2.25) x 3.6 = _____ pounds per day (resid. only)

Trip Generation _____ ADT; _____ added p.m. peak hour, peak direction trips

Stormwater (See the Public Works Comment Sheet.) Does the project meet water quality and water quantity LOS Standards, according to the Public Works Department?

Recreation _____ Does the project degrade the City's adopted LOS Standards for recreation?

Mass Transit _____ Does the project impact any of the City's adopted LOS Standards for mass transit?

Estimated credits for demolition/redevelopment/re-use:

Potable water (_____ units x 2.25) x 200 = _____ peak gallons per day (resid. only)

Water Supply _____ (see GRU)

Wastewater (_____ units x 2.25) x 113 = _____ average gallons per day (resid. only)

Solid Waste (_____ units x 2.25) x 3.6 = _____ pounds per day (resid. only)

Trip Generation _____ ADT; _____ peak p.m. hour, peak direction trips

Note: 2.25 = 2010 Census persons/household in Gainesville, FL

STAFF USE ONLY

This development meets all relevant Planning and Development Services Department LOS standards for concurrency and TMPA Review. Please see the Public Works comment sheet for information about Stormwater Management concurrency.

Signed _____

Date _____

Concurrency long form--nf
Revised: 10/24/13

GAINESVILLE APPLICATION FOR TRAFFIC STUDY REVIEW

OFFICE USE ONLY	
Petition No. _____	Fee: \$ _____
1 st Step Mtg Date: _____	EZ Fee: \$ _____
Methodology Mtg Date: _____	Receipt No. _____
Tax Map No. _____	
Account No. 001-800-8050-4061	

CHECK ONE:

Minor Major **Submittal:** 1st 2nd 3rd

The traffic study review fee includes a maximum of three reviews per development plan.

Owner(s) of Record (please print)	Applicant(s)/Agent(s) (please print)
Name: Jeff Montgomery Associates, LLC	Name: Boos Development Group/CHW
Address: 2357 NW 13th Place	Address: 132 NW 76th Drive
Gainesville, Fl 32605	Gainesville, Fl 32607
E-mail: contact agent	E-mail: hollys@chw-inc.com
Phone: Fax:	Phone: 352 331-1976 Fax: 352 331-2476
<i>(If additional owners, please include on back)</i>	
PROJECT INFORMATION	
Project Name	NW 43rd/53rd Retail Development
New Peak Hour Trips	AM = 147 trips
	PM = 94 trips
Proposed Uses <small>(list type; ITE land use code and number of units or square footage as appropriate)</small>	Bank
	Restaurant
	Medical facility
Location and access	NW 43rd , NW 43rd and NW 53rd

I certify that the above statements are correct and true to the best of my knowledge.


 Applicant's signature

4/4/10
 Date

Certified Cashier's Receipt:



TEL: (352) 331-1976 132 NW 76th Drive, Gainesville, Florida 32607
TEL: (352) 414-4621 101 NE 1st Avenue, Ocala, Florida 34470
WWW.CHW-INC.COM

planning.surveying.engineering.construction.

MEMORANDUM

To: Neighbors of NW 43rd and NW 53rd Retail Development PN 15-0490
From: Holly Simon, EI
Date: January 15, 2016
RE: Neighborhood Workshop Notice

CHW will hold a neighborhood workshop to discuss a proposed development plan located on Alachua County Tax Parcel 06023-001-000, located on the northwestern corner of the intersection of NW 43rd Street and NW 53rd Avenue. The proposed development plan seeks approval of approximately 11,600 sf of commercial development.

This is not a public hearing. The purpose of the workshop is to inform neighboring property owners of the proposal's nature and to seek comments. We look forward to seeing you at the workshop.

Date: Wednesday, February 10, 2016

Time: 6:00 p.m.

Place: Alachua County Library
Millhopper Branch
3145 NW 43rd Street
Gainesville, Florida 32606

Contact: Holly Simon, EI
(352) 331-1976



Property Search Results

The data displayed is the most current data available to the Property Appraiser.

Search Date: 12/4/2015 at 11:06:13 AM' - Data updated: 12/04/15

Printer Friendly Page

Parcel: 06023-001-000 [GIS Map](#)

<p>Taxpayer: JEFF MONTGOMERY ASSOCIATES LLC</p> <p>Mailing: 2357 NW 13TH PL Gainesville, FL 32605</p> <p>Location:</p> <p>Sec-Twn-Rng: 15-9-19</p> <p>Use: Municipal</p> <p>Tax Jurisdiction: Gainesville 3700</p> <p>Area: Sec 1-36 OF 9-19</p> <p>Subdivision: Placeholder</p>	<p>Legal: COM AT SE COR RUN W 420 FT N 26 DEG 51 MIN E 929.80 FT S 829.6 FT TO POB AS PERORDER OF TAKING C-201-70 LESS E 50THEREOF PER OR 1151/192) LESS R/W AS PEROR 1464/0576) OR 4324/1743</p>
--	--

Assessment History

*** Exempt Amount and Taxable Value History reflect County Amounts. School Board and City Amounts may differ. ***

Year	Use	Land	JV Land	Building	Misc	Just Value	Deferred	Assessed	Exempt**	Taxable**	Taxes
2015	Municipal	559700	559700	0	37400	597100	0	597100	0	597100	14132.52
2014	Municipal	559700	559700	0	37400	597100	0	597100	0	597100	14234.74
2013	Municipal	51400	51400	0	1134700	1186100	781230	404870	404870	0	0
2012	Municipal	51400	51400	0	1134700	1186100	818030	368070	368070	0	0
2011	Municipal	51400	51400	0	1134700	1186100	851490	334610	334610	0	0
2010	Municipal	51400	51400	0	1134700	1186100	881906	304194	304194	0	0
2009	Municipal	51400	51400	0	1168700	1220100	943560	276540	276540	0	0
2008	Municipal	51400	51400	0	200000	251400	0	251400	251400	0	0
2007	Municipal	51400	51400	0	200000	251400	0	251400	251400	0	0
2006	Municipal	51400	51400	0	200000	251400	0	251400	251400	0	0
2005	Cty Inc Nonmuni	51400	51400	0	200000	251400	0	251400	251400	0	0
2004	Cty Inc Nonmuni	51400	51400	0	200000	251400	0	251400	251400	0	0
2003		51400	51400	0	200000	251400	0	251400	251400	0	0

	Cty Inc Nonmuni										
2002	Cty Inc Nonmuni	51400	51400	0	200000	251400	0	251400	251400	0	0
2001	Cty Inc Nonmuni	51400	51400	0	200000	251400	0	251400	251400	0	0
2000	Cty Inc Nonmuni	51400	51400	0	200000	251400	0	251400	251400	0	0
1999	Cty Inc Nonmuni	51400	51400	0	200000	251400	0	251400	251400	0	0
1998	Cty Inc Nonmuni	80000	80000	0	200000	280000	0	280000	280000	0	0
1997	Cty Inc Nonmuni	80000	80000	0	200000	280000	0	280000	0	280000	7965.92

Land

<u>Use</u>	<u>Zoning</u>	<u>Acres</u>
Municipally Owned	Commercial (MU1)	2.57
2015 Certified Market Land Value: 559700		2015 Certified Ag Land Value: 559700

Building

<p><u>Actual Year Built</u> 1974</p> <p><u>Effective Year Built</u> 2000</p> <p><u>Use:</u> No Value</p> <p><u>Bedrooms:</u> 0</p> <p><u>Baths:</u> 9</p> <p><u>Stories:</u> 1</p> <p><u>Exterior Wall:</u> Common Brick</p> <p><u>AC:</u> Central Air</p> <p><u>Heating:</u> Forced Air Duct</p>	<p><u>Area Type</u></p> <p>Base Area (BAS) 11275</p> <p>Canopy (CAN) 225</p> <p>Heated Area: 11275 Total Area: 11500</p> <p>Having Trouble Viewing This?</p> <p>Enlarge Drawing / Show Dimensions</p>	<p><u>Square Footage</u></p>
2015 Certified Building Value: 0		

Miscellaneous

<u>Description</u>	<u>Units</u>
Fence BR	1264
Gutter	320
Paving 2	16950
Fence CL	1744
Drive/Walk	408
Bldg Sal	1
2015 Certified Miscellaneous Value: 37400	

Sale

Official Public Records information is provided by the Alachua County Clerk's Office. Clicking on these links will direct you to their web site displaying the document details for this specific transaction.

<u>Date</u>	<u>Price</u>	<u>Vacant</u>	<u>Qualified</u>	<u>OR Book</u>	<u>OR Page</u>	<u>Instrument</u>	<u>OR Link (Clerk)</u>
01/13/2015	1600000	No	No	4324	1743	Special Warranty Deed	Official Public Record

[Link to TaxCollector Record](#)

ReDisplay

Don't Show Building Drawing

[See Help about Building Drawings](#)



BUILD & PRICE

GET OFFERS

SouthernFordDealers.com

The information that is supplied by the Alachua County Property Appraiser's office is public information data and must be accepted and used with the understanding that the data was collected primarily for the use and purpose of creating a Property Tax Roll per Florida Statute. The Alachua County Property Appraiser's Office will not be held liable as to the validity, correctness, accuracy, completeness, and / or reliability of this data. The Alachua County Property Appraiser's Office furthermore assumes no liability whatsoever associated with the use or misuse of this public information data.

Alachua County Property Appraiser • 515 N Main Street • Gainesville, FL 32601 • 352-374-5230 (FAX) 352-374-5278

SIGN IN SHEET

Project Number: 15-0490

Meeting: Neighborhood Workshop Meeting

Date: February 10, 2016

Time: 6pm

Project Name: NW 43rd and NW 53rd Retail Development

Name	Company Name	Email	Phone Number
Charles Hall	-	howitser.cannon@ ^{yahoo} com	352-326-1006
Roxanne Watkins	Ashton	RFWATKINS@cox.net	375-0603
Richard Watkins		Rich Watkins@cox.net	375-0603
Cheryl Berry	Ashton	lmuchadu@cox.net	371-9268
Richard Aurorecht	Ashton	Aurorecht.R@GMAIL.COM	318-0589
Eleanor Snyder	Ashton	-	-
George Snyder	ASHTON	g_snyder@BellSouth.net	372-9872
Charles R. Fellows	"	cfellows@waterindustrial.com	
Steve Mitchell	Worthington Industries	worthington@psychoc.com	352-371-3604
Don West	ASHTON	donwest9@hotmail.com	352-373-8812
Beth Newsom	Ashton	thenewsom5@bellsouth.net	222-2647
Rich Cain	Ashton/Front Street	rich@frontstreet.net	352-281-9393
DAVIESMOCK	ASHTON	dsmock75@BELL SOUTHERN.NET	
Leo Nico	Ashton	nicoleg@cox.net	377-8781
Jeff Montgomery	-	-	-
Gina Duncan	-	g_gduncan@msu.com	-
Barbara Laey	Ashton	butsybab@cox.net	(352)378-2669
Alex & Charles Lee	ASHTON SONS	calee1@cox.net	(352)373-6886

Name	Company Name	Email	Phone Number
Dan Drotos	Bosshardt Realty	dan@teamyds.com	(954) 551-9846
Angelica Hudson	N/A	angelotmodel@yahoo.com	937 901-9069
Bennie Hudson	—	—	937 902 3343
Marie Vanasse	—	—	574 261 1762
Christy Montgomery	Montgomery Associates	hkam@att.net	352-538-9735
Teny Kamm	Ashton	hkam@att.net	352-346-8926

FLORIDA DEPARTMENT OF STATE
DIVISION OF CORPORATIONS**Detail by Entity Name****Florida Profit Corporation**

BOOS DEVELOPMENT GROUP, INC.

Filing Information

Document Number	P97000097926
FEI/EIN Number	59-3483138
Date Filed	11/18/1997
State	FL
Status	ACTIVE
Last Event	AMENDMENT
Event Date Filed	07/17/2013
Event Effective Date	NONE

Principal Address410 Park Place Boulevard
Suite 100
CLEARWATER, FL 33759

Changed: 02/16/2016

Mailing Address410 Park Place Boulevard
Suite 100
CLEARWATER, FL 33759

Changed: 02/16/2016

Registered Agent Name & AddressBOOS, ROBERT B
410 Park Place Boulevard
Suite 100
CLEARWATER, FL 33759

Name Changed: 04/14/2003

Address Changed: 02/16/2016

Officer/Director Detail**Name & Address**

Title DCCO

BOOS, ROBERT D
 410 Park Place Boulevard
 Suite 100
 CLEARWATER, FL 33759

Title PSD

BOOS, ROBERT B
 410 Park Place Boulevard
 Suite 100
 CLEARWATER, FL 33759

Title V

WRIGHTENBERRY, E. GENE
 410 Park Place Boulevard
 Suite 100
 CLEARWATER, FL 33759

Annual Reports

Report Year	Filed Date
2014	03/21/2014
2015	02/11/2015
2016	02/16/2016

Document Images

02/16/2016 -- ANNUAL REPORT	View image in PDF format
02/11/2015 -- ANNUAL REPORT	View image in PDF format
03/21/2014 -- ANNUAL REPORT	View image in PDF format
07/17/2013 -- Amendment	View image in PDF format
03/05/2013 -- ANNUAL REPORT	View image in PDF format
01/04/2012 -- ANNUAL REPORT	View image in PDF format
01/12/2011 -- ANNUAL REPORT	View image in PDF format
01/14/2010 -- ANNUAL REPORT	View image in PDF format
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02/24/2006 -- ANNUAL REPORT	View image in PDF format
01/05/2005 -- ANNUAL REPORT	View image in PDF format
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04/14/2003 -- Reg. Agent Change	View image in PDF format
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03/12/2001 -- ANNUAL REPORT	View image in PDF format
04/11/2000 -- ANNUAL REPORT	View image in PDF format

[05/06/1999 -- ANNUAL REPORT](#)

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[11/18/1997 -- Domestic Profit](#)

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06023-100-009
BOSSHARDT PROPERTIES LLC
5542 NW 43RD ST
GAINESVILLE FL 32653

06023-100-010
CASSINI RENTALS LLC
6424 NW 85TH TER
Gainesville FL 32653

06070-010-001
MONTGOMERY JEFFREY M
5221 NW 43RD ST
GAINESVILLE FL 32606-4454

06070-010-002
ARC CVGNVFL001 LLC
% CVS # 4139-02/STORE ACCT
1 CVS DRIVE
WOONSOCKET RI 02895

06023-100-001
AREY & WILLIAMS CO-TRUSTEES
3520 NW 31ST ST
GAINESVILLE FL 32605

06023-100-002
WITT WILLIAM M
5622 NW 43RD ST
GAINESVILLE FL 32653-8390

06023-100-003
BENTON & BENTON
3641 NW 23RD PL
GAINESVILLE FL 32605-2621

06023-100-004
BOSSHARDT PROPERTIES LLC
5532 NW 43RD ST
Gainesville FL 32653

06023-100-011
WATERFORD EQUITIES INC
537 NE 1ST ST STE 5
GAINESVILLE FL 32601

06023-100-006
BARBARETTE & QUIRK
5538 NW 43RD ST #A
GAINESVILLE FL 32653-3301

06023-100-007
BOSSHARDT REALTY SERVICES INC
5542 NW 43RD ST
GAINESVILLE FL 32653-8333

06023-100-008
WORTHINGTON DWIGHT A
5528 NW 43RD ST
GAINESVILLE FL 32653-3301

06023-001-000
JEFF MONTGOMERY ASSOCIATES LLC
2357 NW 13TH PL
Gainesville FL 32605

06020-001-000
CITY OF GAINESVILLE ,
% LAND RIGHTS COORDINATOR
PO BOX 490 MS 58
GAINESVILLE FL 32627

06023-040-071
WHIDDON NANCY SUE
4515 NW 53RD LN
Gainesville FL 32653

06023-040-070
BALL CURTIS E
4514 NW 53RD LN
GAINESVILLE FL 32653

06023-040-062
RUWE JONATHAN
19170 SWEIG TER
LEESBURG VA 20176-3851

06023-040-061
RUSH JACOB A
5330 NW 45TH DR
GAINESVILLE FL 32653

06023-040-060
NEWSOM CHARLES IRWIN
5320 NW 45TH DR
GAINESVILLE FL 32653

06023-040-057
BURLESON GORDON S
5331 NW 45TH DR
GAINESVILLE FL 32653-8300

06023-040-056
CAIN PATRICK E
5403 NW 45TH DR
GAINESVILLE FL 32653-8300

06023-040-055
LACY BARBARA A
5413 NW 45TH DR
GAINESVILLE FL 32653-8300

06023-040-054
DEPAZ OSCAR B
5423 NW 45TH DR
GAINESVILLE FL 32653-8300

06053-001-002
WACHOVIA BANK
% THOMSON REUTERS
PO BOX 2609
CARLSBAD CA 92018

06023-040-059
HALL CHARLES M TRUSTEE
5310 NW 45TH DR
Gainesville FL 32653

06023-011-000
WATERFORD PARK INC
5528 NW 43RD ST
GAINESVILLE FL 32653-3301

06053-001-000
HYDRA CORPORATION
200 W FORSYTH ST
JACKSONVILLE FL 32202

06023-100-005
ROSCOW JOHN F IV
5608 NW 43RD ST
GAINESVILLE FL 32653

06023-040-058
BENSON NEAL
6225 NW 52ND TER
Gainesville FL 32653

06070-010-005
POSSUM CREEK OWNER'S ASSOC INC
101 SW 23RD TER
GAINESVILLE FL 32607-3120

06053-009-001
STERLING PARK OWNERS ASSOC INC
6110 NW 1ST PL #B
C/O ACTION REAK ESTATE
GAINESVILLE FL 32607

06023-040-074
ASHTON HOMEOWNERS ASSOC INC
%ASSOCIATION MANAGEMENT
5200 NW 43RD ST STE 102-217
GAINESVILLE FL 32602-4482

April 5, 2016

City of Gainesville
306 NE 6th Ave
Gainesville, FL 3260

Ref: NW 43rd Street/NW 53rd Ave Retail Development
Gainesville, FL

Dear Staff:

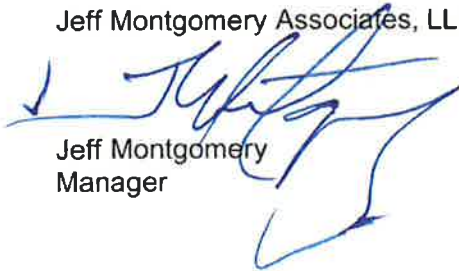
I have authorized Boos Development Group and CHW, Inc. to apply for and obtain development orders/permits for the NW 43rd Street/NW 53rd Ave Retail Development Site located on Tax Parcel 06023-001-000, located northwest of the intersection of NW 43rd Street and NW 53rd Avenue in Gainesville, FL.

Furthermore, I also authorize CHW to act as agent in regards to any submittals or questions you may have for this project.

Please contact Holly Simon, EI with CHW, Inc. at 352-331-1976 with any questions or concerns you may have about this project.

Sincerely,

Jeff Montgomery Associates, LLC



Jeff Montgomery
Manager

The foregoing affidavit is acknowledged before me this 29 day of March, 2016 by Jeff Montgomery, who is/are personally known to me, or who has/have produced personally know as identification.

NOTARY SEAL



Signature of Notary Public, State of FL



Attachment “D”

Comprehensive Plan and Land Development Code References.

Comprehensive Plan Goals, Objectives and Policies

This petition addresses a request to construct a gasoline service station with a convenience store, located within the MU-2 zoning district. The Land Development Code requires that all gas

Objective 10.3 The City's Land Development Code shall provide design standards for all new developments and redevelopment within the TMPA.

Policy 10.3.1 The City shall use the Central Corridors Overlay District design standards in the Land Development Code for development/redevelopment projects within the TMPA. These standards address building placement, parking, sidewalks, building wall articulation, and placement of mechanical equipment, and shall be the guiding design standards for development/redevelopment on roadways in the TMPA that are listed in the annual Level of Service Report produced by the North Central Florida Regional Planning Council. Within Zones C and M, the build-to line may be modified on Archer Road, SW 34th Street, SW 20th Avenue, or Williston Road due to right-of-way or utility constraints, consistent with requirements as described in the Land Development Code's Special Area Plan for Central Corridors. These design standards shall not supersede design standards adopted as part of a Special Area Plan, Overlay District, Planned Development, or Urban Mixed-Use District 2 (UMU-2) zoning district.

Policy 10.3.2 New development of automotive-oriented uses within the TMPA, such as retail petroleum sales (gasoline service stations), car washes, automotive repair, and limited automotive services (as defined in the Land Development Code), shall be designed with service bays and fueling (gas) pumps located to the rear of buildings. These design standards shall not apply in industrial zoning districts. The number of fueling positions shall be regulated by TMPA policies.

Objective 10.4 Automobile-oriented developments/uses within the TMPA, including drive-through facilities, surface parking lots as a principal use, parking garages, car washes, and gasoline service stations, shall be regulated as follows.

Policy 10.4.1 The City may establish pedestrian, transit, and bicycle-oriented areas, through a special area plan overlay zone adopted within the Land Development Code, to prohibit or further regulate automobile-oriented developments/uses beyond the standards set by the TMPA.

Policy 10.4.2 Special area plan overlay district regulations (such as the College Park Special Area Plan and the Traditional City) that prohibit and regulate automobile-oriented developments/uses, as described in Objective 10.4, shall not be modified by provisions or policies of the TMPA.

Policy 10.4.3 New development of surface parking lots as a principal use shall be required to obtain a Special Use Permit. In addition to the review criteria set in the

Land Development Code for Special Use Permits, the approval of the Special Use Permit shall be based on consideration of the size/scale of the proposed surface parking lot and the inclusion of design and access features that maintain pedestrian, bicycle, and transit safety and do not discourage pedestrian, bicycle, and transit use in the area.

Policy 10.4.4 Drive-through facilities shall be defined to include banking facilities, payment windows, restaurant, food and/or beverage sales, dry cleaning, express mail services, and other services that are extended mechanically or personally to customers who do not exit their vehicles. The following uses shall not be considered drive-throughs: auto fuel pumps and depositories that involve no immediate exchange or dispersal to the customer, such as mail boxes, library book depositories, and recycling facilities.

In addition to the review criteria set in the Land Development Code for Special Use Permits, the approval of a drive-through facility shall be based on the following criteria:

- a. maximization of pedestrian and bicycle safety and convenience;
- b. adequate queuing space for vehicles such that there is no back-up of traffic onto adjacent roadways;
- c. provision of a by-pass lane or sufficient driveway area around the drive-through lanes to assist internal vehicular circulation;
- d. minimization of the visual impacts of the drive-through lanes on street frontage areas;
- e. minimization of the total number of drive-through lanes based on site conditions and the operating conditions of the impacted roadway segments;
- f. minimization of the number of access points to roadways;
- g. design of access points and ingress/egress directional flows to minimize impacts on the roadway and non-motorized traffic;
- h. design of internal pedestrian access and safety as related to the position of the drive-through lane(s); and
- i. meeting any additional design criteria established in the Land

Development Code.

Policy 10.4.5 Unless otherwise prohibited or regulated by a special area plan, the development of new free-standing drive-through facilities or expansion of or development activity at existing free-standing drive-through facilities not meeting the provisions of Policy 10.4.6 shall be required to obtain a Special Use Permit. These drive-through facilities shall meet the Special Use Permit criteria in the Land Development Code and review criteria shown in Policy 10.4.4. In addition, drive-through facilities not developed under the provisions of Policy 10.4.6 or 10.4.7 shall also meet the following standards:

- a. There shall be a minimum distance of 400 feet between the driveways of sites with free-standing drive-through facilities on roadways operating at 85% or more of capacity. Roadway capacity shall be measured using the latest version of Art-Plan or a method deemed acceptable by the Technical Advisory Committee Subcommittee of the Metropolitan Transportation Planning Organization. Available capacity shall include consideration of reserved trips for previously approved developments and the impacts of the proposed development. The 400 feet distance requirement shall not apply if any of the following criteria are met:
 1. Joint driveway access or common access is provided between the sites with free-standing drive-through facilities;
 2. Cross access is provided with an adjoining property;
 3. A public or private road intervenes between the two sites; or
 4. The development provides a functional design of such high quality that the pedestrian/sidewalk system and on-site/off-site vehicular circulation are not compromised by the drive-through facility. This determination shall be made as part of the Special Use Permit and development plan review process and shall be based on staff and/or board review and approval.
- b. There shall be no credit for pass-by trips in association with the drive-through facility. Criteria that must be met for any of the zones shall be based on total trip generation for the use and shall not include any net reduction for pass-by trips.

Policy 10.4.6 Unless otherwise prohibited or regulated by a special area plan, new development or expansion of free-standing drive-through facilities shall be permitted, by right, only within shopping centers or mixed-

use centers. No direct access connections from the street to the drive-through shall be allowed. Access to the drive-through shall be through the shopping center or mixed-use center parking area. Mixed-use centers shall be defined as developments that are regulated by a unified development plan, consist of three or more acres, have a minimum of 25,000 square feet of gross floor area, provide centralized motorized vehicle access, and include a mix of at least three uses that may include residential or non-residential uses in any combination. Mixed-use centers may include Planned Developments that meet the criteria listed in this policy. Development plan approval for the drive-through facility shall be based on the inclusion of appropriate pedestrian, bicycle, and transit features that facilitate and encourage convenience, safety, and non-motorized use of the site; design of safe internal pedestrian access as related to the position of the drive-through lane(s); and meeting of design criteria established in the Land Development Code. Drive-through facilities meeting the criteria shown in this policy shall also receive an internal capture trip credit and credit for pass-by trips.

Policy 10.4.7

New development of a drive-through facility may be permitted, by Special Use Permit, when it will be part of a single mixed-use building that is at least 25,000 square feet and that has more than one business or use at the site. Only one drive-through use at such site shall be allowed. In addition to the review criteria in the Land Development Code for Special Use Permits and the review criteria in Policy 10.4.4, the approval of the Special Use Permit shall be based on the inclusion of pedestrian, bicycle, and transit features that facilitate and encourage convenience, safety, and non-motorized use of the site; design of safe internal pedestrian access as related to the position of the drive-through lane(s); and meeting of design criteria established in the Land Development Code. Drive-through facilities meeting the criteria shown in this policy shall also receive an internal capture trip credit and credit for pass-by trips.

Future Land Use Element

Objective 1.2

Protect and promote viable transportation choices (including transit, walking and bicycling, and calmed car traffic).

Policy 1.2.5

The City should encourage creation of short-cuts for pedestrians and bicyclists with additional connections and cross access in order to create walking and bicycling connections between neighborhoods and neighborhood (activity) centers.

Policy 1.2.7

The City should strive, incrementally, and when the

opportunity arises street by street to form an interconnected network of neighborhood streets and sidewalks supportive of car, bicycle, pedestrian, and transit routes within a neighborhood and between neighborhoods knitting neighborhoods together and not forming barriers between them. Dead ends and cul-de-sacs should be avoided or minimized. Multiple streets and sidewalks should connect into and out of a neighborhood.

Policy 1.5.4 The City should prioritize core areas to receive the first enhancements when the City has planned citywide public improvements such as sidewalks, street re-paving, undergrounding utilities, street lights, and public

Objective 1.3

Adopt land development regulations that guide the transformation of conventional shopping centers into walkable, mixed-use neighborhood (activity) centers.

Objective 1.5

Discourage the proliferation of urban sprawl.

GOAL 2

REDEVELOP AREAS WITHIN THE CITY, AS NEEDED, IN A MANNER THAT PROMOTES QUALITY OF LIFE,

Objective 2.1

Redevelopment should be encouraged to promote compact, vibrant urbanism, improve the condition of blighted areas, discourage urban sprawl, and foster compact development patterns that promote transportation choice.

Policy 2.1.1

The City shall develop recommendations for areas designated as redevelopment areas, neighborhood centers and residential neighborhoods in need of neighborhood enhancement and stabilization.

b. The City should include in its redevelopment plans recommendations regarding economic development strategies, urban design schemes, land use changes, traffic calming, and infrastructure improvements;

c. The City should identify potential infill and redevelopment sites; provide an inventory of these sites; identify characteristics of each parcel, including land development regulations, infrastructure availability, major site limitations, and available public assistance; and develop a strategy for reuse of these sites; d. The City should encourage retail and office development to be placed close to the street side sidewalk.

Objective 4.2

The City shall implement regulations that will protect low-intensity uses from the negative impacts of high-intensity uses and provide for the healthy coexistence and integration of various land uses.

Objective 4.2

The City shall implement regulations that will protect low-intensity uses from the negative impacts of high-intensity uses and provide for the healthy coexistence and integration of various land uses.

Policy 4.2.1

The City shall adopt land development regulations that provide protection for adjacent residential areas and low intensity uses from the impacts of high intensity uses by separating intense uses from low - intensity uses by transitional uses and by performance measures. Performance measures shall address the buffering of adjacent uses by landscape, building type and site design. Regulation of building type shall insure compatibility of building scale, and overall building appearance in selected areas. Regulation of site design shall address orientation. Such regulation shall also include arrangement of functions within a site, such as parking, loading, waste disposal, access points, outdoor uses and mechanical equipment; and the preservation of site characteristics such as topography, natural features and tree canopy.

Policy 4.2.2

The City shall adopt land development regulations that encourage better access between residential neighborhoods and adjacent neighborhood centers through the use of street design and the use of pedestrian, bicycle and transit modifications.

Policy 4.2.3

The existence of non-residential uses on one or more corners of an intersection will not justify approval of the development of all corners with the same or similar use, nor does the existence of non-residential uses on a major arterial street dictate that all frontage must be similarly used.

Objective 5.1 **Urban design standards established in the Land Development Code shall enhance the sense of place, improve the urban form, and provide for the safety and comfort of pedestrians, bicycles, transit, and other vehicles in the City. These standards shall reflect a commitment to improve and maintain the vitality of the City and its neighborhoods.**

LAND DEVELOPMENT CODE REFERENCES

Sec. 30-64. - Mixed use low intensity district (MU-1).

- (a) Purpose. The mixed-use low intensity district is established for the purpose of allowing coordinated developments designed to offer a mixture of residential, convenience-type retail, professional and consumer service uses primarily for residents of mixed-use and adjacent residential neighborhoods, and places of religious assembly. The district is intended to reduce the length and number of vehicular trips by providing for basic needs within close proximity to residential areas, by encouraging pedestrian access, and by the combining of trips. This district is established to allow uses compatible with each other and with surrounding residential areas to be developed near each other. The MU-1 district may be located in areas where analysis of residential characteristics demonstrates that such facilities are required. This district is intended to encourage the development of planned and unified neighborhood shopping centers in a relationship harmonious with adjoining residential activities. It is also intended to accommodate traditional neighborhoods that include nonresidential uses and neighborhood centers.
- (b) Objectives. The provisions of this district are intended to:
- (1) Permit compatible commercial, office, service and residential developments that benefit from being located near each other.
 - (2) Provide an adequate mix of residential uses including multifamily, townhouse, zero lot line, and detached single-family at urban densities.
 - (3) Provide opportunities for the development of compound residential uses.
 - (4) Minimize traffic congestion by:
 - a. Requiring that shopping center and/or mixed-use developments be located on appropriate major collector and arterial roadways, as defined in the comprehensive plan;
 - b. Minimizing the number and regulating the location of driveway connections; and
 - c. Encouraging pedestrian and nonautomotive access.
 - (5) Ensure, through development plan approval, that nonresidential and mixed-use developments are designed to promote the most efficient use of the land, and that they coordinate the internal activities of the site as well as establish a harmonious relationship between such developments and their environment.
 - (6) Require buffering or screening around nonresidential and/or mixed-use development in accordance with the land development code when the development abuts any property zoned for residential use or shown as residential on the future land use map.
 - (7) Accommodate neighborhood-level services and retail uses along existing business corridors.
 - (8) Coordinate the location and size of mixed-use developments commensurate with the character and density of the areas to be served.
 - (9) Allow the market some flexibility in determining locations of new nonresidential development, and the ability to expand such areas in relation to the population densities achieved.
 - (10) Encourage nonresidential and/or mixed-use developments to locate on land that is physically capable of supporting the particular type of development.
- (c) Permitted uses. See permitted uses listed in subsection (g) of this section pertaining to permitted uses.
- (1) Specific conditions for residential uses. If MU-1 zoning abuts a single-family residential zoning district, then the density of the residential portion of the mixed-use development shall be limited to that allowed by the RMF-6 residential district in the area within 100 feet of the property line, plus the required buffers for that single-family residential zoning district. In addition, multi-family development shall comply with all regulations in the RMF-6 district and the requirements of section 30-56.

- (2) Specific conditions for single-family compound uses. Twenty-five percent of the total floor area up to 1,000 square feet may be used for commercial or office uses. Such uses shall require a minimum lot size of 6,000 square feet and a lot width of 70 feet and shall have direct or shared access to a collector or arterial roadway.
- (d) Specific conditions for neighborhood shopping centers.
- (1) Developments of more than 30,000 square feet. There is no minimum size for buildings within the MU-1 zoning district. However, within the MU-1 zoning district, nonresidential developments of more than 30,000 square feet of gross leasable floor area are considered neighborhood shopping centers and are subject to the rights of and conditions for neighborhood shopping centers.
 - (2) Location. Neighborhood shopping centers shall be located within one-fourth mile of intersections of arterials or intersections of arterials and collectors, as shown on the map entitled Functional Classification of Streets, in the transportation mobility element of the city's comprehensive plan. Such uses shall have direct or shared access to an arterial.
 - (3) Maximum gross leasable nonresidential floor area. No more than 100,000 square feet of gross leasable nonresidential floorspace shall be allowed within any neighborhood shopping center.
 - (4) Maximum gross leasable nonresidential floor area in any one business. No more than 50,000 square feet shall be contained in any one business located within a neighborhood shopping center, except MG-54 (food stores).
 - (5) Dimensional requirements for permitted nonresidential uses. All principal and accessory structures shall be located and constructed in accordance with the following requirements:
 - a. Required yard setbacks:
 1. Minimum front: 20 feet.
 2. Maximum front: 80 feet.
 3. Where the side or rear yard abuts property which is in a residential zoning district, or is shown for residential use on the future land use map of the comprehensive plan, the minimum setback shall be 50 feet or the distance created by the 45 degree angle of light obstruction, whichever is greater.
 - b. Maximum lot coverage: 50 percent for single-purpose, nonresidential; 60 percent for mixed-use (inclusive of residential) or compound use.
 - c. Maximum building height: Five stories.
 - (6) Multiple structures. The use of multiple structures shall be considered on a case-by-case basis during development plan approval. Approval shall be conditioned upon findings by the appropriate reviewing board that all such structures are compatible with the uses and purposes of the center and surrounding uses and traffic patterns and are safely incorporated into the overall transportation system for the center.
 - (7) Outparcels. The proliferation of outparcels contributes to strip commercial development, traffic circulation problems and visual clutter, and obstructs pedestrian and bicycle movement. To mitigate the problems associated with outparcels, the following regulations shall apply:
 - a. Creation and design. The creation of outparcels shall be considered on a case-by-case basis during subdivision, lot split approval or development plan approval. Approval shall be conditioned upon findings by the appropriate reviewing board or staff, as applicable, that the neighborhood shopping center and all outparcels are integrated through the use of landscaping and buffers; shared parking, traffic access and circulation; and stormwater management.
 - b. Dimensional requirements for outparcels. Outparcels which have unified circulation systems with the adjoining neighborhood shopping center shall not be required to meet the minimum lot area, width and depth requirements; however, development on outparcels

shall be required to meet yard setback, lot coverage and building height requirements for the MU-1 district.

(8) Access.

- a. Vehicular access. Access to the neighborhood shopping center shall be in accordance with the provisions of article IX, division 3, and chapter 23 of the Code of Ordinances. Parking areas, including maneuvering space, ingress and egress roads and driving lanes, shall be improved in accordance with the provisions of article IX, division 3, and chapter 23 of the Code of Ordinances. All loading and unloading shall be done on the neighborhood shopping center property. Areas used by motor vehicles shall be physically separated from public streets by landscaped buffer areas.
- b. Bicycle, greenway and pedestrian access. Provisions shall be made to safely incorporate travel ways for bicycle and pedestrian usage into any neighborhood shopping center project. Where bikeways, greenways or sidewalks are presently adjoining the property, provisions shall be made to safely link the internal bicycle and pedestrian system with adjoining facilities. During development plan review, the appropriate reviewing board shall also review the relationship of the neighborhood shopping center to adjoining properties and may require appropriate access for bicycles or pedestrians at locations where vehicular access is prohibited.

(e) Dimensional requirements for projects of less than 30,000 square feet.

(1) Yard setbacks:

- a. Front: The front setback shall be no deeper than the average setback of existing development in the same block face, and within 15 to 80 feet.
- b. Where the side or rear yard abuts property which is in a residential district or is shown for residential use on the future land use map of the comprehensive plan, the minimum setback shall be 25 feet or the distance created by the 60-degree angle of light obstruction, whichever is greater.
- c. Where the property abuts a side street, the minimum setback from that street shall be ten feet.
- d. Where a nonresidential use is adjacent to a nonresidential use no side yard setback is required.

(2) Accessory structures shall not exceed 25 feet in height.

(3) Maximum lot coverage: 50 percent for single-purpose, nonresidential; 60 percent for mixed-use (inclusive of residential) or compound use.

(4) Maximum building height: Five stories.

(f) Access. Access shall be designed to integrate all aspects of the development and shall meet the requirements of article IX, division 3, of this chapter. Driveways and parking shall be coordinated or shared insofar as possible.

(g) Permitted uses.

SIC	Uses	Conditions
	USES BY RIGHT:	
	Any accessory uses customarily and clearly incidental to any permitted	

	principal use	
	Bed and breakfast establishments	In accordance with article VI
	Community residential homes with 14 or fewer residents	When part of a permitted single-family or multifamily residential component and in accordance with article VI
	Community residential homes with more than 14 residents	In accordance with article VI
	Compound uses	
	Eating places	
	Food trucks	In accordance with article VI
	Family child care homes	When part of a permitted single-family or multifamily residential component and in accordance with state law
	Large family child care homes	In accordance with article VI
	Outdoor cafes	As defined in article II and in accordance with article VI
	Personal fitting and sales of prosthetic or orthopedic appliances	
	Places of religious assembly	In accordance with article VI
	Public service vehicles	As defined and in accordance with article VI
	Repair services for household needs	As defined in article II
	Residential (8 to 30 dwelling units per acre)	In accordance with the requirements of the RMF-6, RMF-7 or RMF-8 zoning districts and the additional requirements of this section, and the requirements of section 30-56. Lots that existed on November 13, 1991 as recorded in the planning and development services department and that are less than or equal to 0.5 acres in size are exempt from minimum density requirements.

	Residential use buffer	
	Neighborhood convenience center	
	Neighborhood shopping center	
	Specialty T-shirt production	
GN-074	Veterinary services	Only within enclosed buildings and in accordance with article VI
IN-0752	Animal specialty services, except veterinary	Only within enclosed buildings
GN-078	Landscape and horticultural services	
MG-15	Building construction - General contractors and operative builders	
GN-171	Plumbing, heating and air conditioning	
GN-172	Painting and paper hangers	
GN-173	Electrical work	
MG-27	Printing, publishing and allied industries	
MG-43	U.S. Postal Service	
GN-472	Arrangement of passenger transportation	Offices only, with no operation of passenger tours from the site
GN-481	Telephone communications	Accessory transmission, retransmission and microwave towers up to and including 100 feet in height in accordance with article VI, excluding cellular telephone services

GN-482	Telegraph and other message communications	Accessory transmission, retransmission and microwave towers up to and including 100 feet in height in accordance with article VI
GN-483	Radio and television broadcasting stations	Accessory transmission, retransmission and microwave towers up to and including 100 feet in height in accordance with article VI
GN-523	Paint, glass and wallpaper stores	
GN-525	Hardware stores	
GN-526	Retail nurseries, lawn and garden supply stores	
MG-53	General merchandise stores	
MG-54	Food stores	Accessory gasoline and alternative fuel pumps in accordance with article VI, and by special use permit in neighborhood convenience and shopping centers
GN-553	Auto and home supply stores	Excluding garage and installation facilities
GN-554	Gasoline service stations including the sale of alternative fuels for automobiles	In accordance with article VI and by special use permit in neighborhood convenience and shopping centers
MG-56	Apparel and accessory stores	
MG-57	Home furniture, furnishings and equipment stores	
MG-59	Miscellaneous retail	Excluding direct selling establishments (IN-5963); fuel dealers (IN-5983); including liquor stores (IN-5921) limited to 2,000 square feet and excluding on-site consumption when located in neighborhood convenience and shopping centers, inside storage, display and sales only for all uses
Div. H	Finance, insurance and real estate	Excluding cemetery subdividers and developers (IN-6553)

MG-72	Personal services	Including funeral services and crematories in accordance with article VI and excluding linen supply (IN-7213), industrial laundries (IN-7218), and diaper services in neighborhood convenience and shopping centers
MG-73	Business services	Excluding outdoor advertising services (IN-7312), disinfecting and pest control services (IN-7342), heavy construction equipment rental and leasing (IN-7353), and equipment rental and leasing, not elsewhere classified (IN-7359)
GN-752	Automobile parking	
MG-76	Miscellaneous repair services	Excluding miscellaneous repair shops and related services (GN-769) and including repair services for household needs as defined in this chapter
MG-78	Motion pictures	Excluding drive-in theaters (IN-7833) and outdoor filming facilities
MG-79	Amusement and recreation services	Excluding go-cart rental and raceway operations, commercial sports (GN-794), and simulated gambling establishments. Must be located within enclosed structures.
MG-80	Health services	Including nursing and intermediate care facilities in accordance with article VI when applicable, and excluding rehabilitation centers and hospitals (GN-806)
MG-81	Legal services	
MG-82	Educational services	Including private schools, in accordance with article VI
MG-83	Social services	Including day care centers as defined in this chapter and in accordance with article VI, excluding residential care (GN-836), rehabilitation centers, halfway houses, social service homes, and residences for destitute people as defined in this chapter
GN-841	Museums and art galleries	
MG-86	Membership organizations	

MG-87	Engineering, accounting, research management and related services	Excluding testing laboratories (IN-8734) and facilities support management services (IN-8744)
MG-89	Services, not elsewhere classified	
Div. J	Public administration	
	USES BY SPECIAL USE PERMIT	
	Accessory gasoline and alternative fuel pumps	In accordance with article VI in neighborhood convenience and shopping centers
	Accessory transmission, retransmission and microwave towers over 100 feet in height	In accordance with article VI
	Alcoholic beverage establishments	In accordance with article VI
	Limited automotive services	In accordance with article VI
	Other uses (including light assembly or packaging)	Within completely enclosed structures; no outdoor storage, truck traffic limited to that normal to commercial activities such as grocery stores, loading docks and mechanical equipment must be screened, and sound attenuation shall be provided to any adjacent residential area or area in actual residential use; no access to any residential street; must meet industrial buffers. Storage of hazardous materials in accordance with the county hazardous materials management code
	Rehabilitation centers	In accordance with article VI
	Residences for destitute people	In accordance with article VI
	Social service homes	In accordance with article VI
GN-598	Fuel dealers	

GN-701	Hotels and motels	Except in neighborhood convenience and shopping centers
GN-702	Roominghouses and boardinghouses	In accordance with article VI

- (h) General requirements. All structures and uses within this district shall also comply with the applicable requirements and conditions of section 30-67 and article IX.

The following lot and building requirements shall apply in all zoning districts:

(1) Frontage on right-of-way.

- a. Minimum property frontage. In all districts except Planned Development District (PD), no building or structure, except as hereinafter provided, shall be erected on a lot or parcel of land which does not physically abut a public or approved private street for the required minimum lot width of the district where the same is located. The city manager or designee may designate a private street as an "approved private street" provided it meets one of the following standards:
1. A perpetual ingress/egress easement recorded in the public records of Alachua County that provides legal access to the lot or parcel of land from a dedicated road or right-of-way and which is deemed capable of carrying public safety vehicles; or
 2. Property owned jointly or in common by all property owners fronting the private street as shown in the public records of Alachua County that connects to a dedicated road or right-of-way and which is deemed capable of carrying public safety vehicles.

For the purposes of this section, two classes of approved private streets shall be designated by the city manager or designee:

1. Existing private streets which were constructed prior to September 26, 1994; and
2. New private streets which are approved for construction after September 26, 1994.

The city manager or designee shall have the authority to approve or disapprove, for the purposes of erection of buildings or structures, private streets which existed prior to September 26, 1994. All private streets approved for construction after September 26, 1994 must meet the construction standards for public streets as shown in the Public Works Design Manual, except in the PD district. In PD districts, the geometric construction standards may be varied as set forth in the planned development ordinance. For the purposes of minor subdivision and lot splits, the approved private street must also meet the requirements in section 30-189.

In zoning districts in which there is no minimum lot width requirement, the lot or parcel of land must abut a public or approved private street for the maximum driveway width dimension requirement [as provided in section 30-336(8)] plus any required turning radii area.

Notwithstanding any provision of this section to the contrary, a single-family dwelling, if it is an allowable use in the district, may be erected on a lot or parcel of land which abuts at

least one public or approved private street for at least 25 feet, provided that the minimum lot width for the district in which it is located is met at the required front yard setback line. Provided, further, that any single-family dwelling existing on a lot which does not conform to the provisions of this subsection (1)a of this section may be modified, enlarged or extended, and/or an accessory building or structure may be added to a single-family dwelling, provided that such modification, enlargement, extension or addition complies with all the other requirements of the district in which the lot is located.

- b. Special exception for single-family dwelling. The board of adjustment may authorize by special exception the issuance of a building permit for a single-family dwelling on a lot that does not meet the minimum property frontage requirements of subsection (1)a. of this section, provided all of the following conditions are met:
 - 1. The owner of the subject lot does not own adjoining land which, when added to the subject lot or dedicated as a public street, would satisfy the minimum requirements of subsection (1)a. of this section;
 - 2. A single-family dwelling is a permitted use in the district in which the lot is located;
 - 3. The dimensional requirements of the district are met;
 - 4. Adequate physical access for all motor vehicles (including emergency vehicles) is provided either by an ingress/egress easement or private street; and
 - 5. The board of adjustment finds that such a building will not create any condition detrimental to the safety, convenience and quiet possession of surrounding properties and uses.

In granting the special exception, the board of adjustment may request the property owner to dedicate property for future street right-of-way purposes as may be recommended by the traffic engineering department.

(2) Minimum setbacks from centerlines of all streets.

- a. Every required minimum front yard setback line shall be established by a line parallel to the centerline of the street right-of-way at the distance listed in this subsection, measured from the street right-of-way line or the distance from the street centerline listed in subsection (2)b. of this section. For the purpose of determining street classifications, the official roadway map shall be used.
- b. If the actual street right-of-way line is closer to the street centerline than the following distances, the setback shall be measured from a line parallel to the street centerline located the following distances from the centerline:
 - 1. Highways and arterials: 50 feet.
 - 2. Thoroughfares and collectors: 40 feet.
 - 3. Minor collectors: 35 feet.
 - 4. All other streets: 25 feet.

(3) Overhanging and protruding projections.

- a. Every part of a required yard or court shall open from its lowest point to the sky, unobstructed except for the customary projection of sills, belt courses, cornices, ornamental features and eaves; provided, however, that none of the above projections shall extend into a required yard more than 36 inches; and further provided that none of the above projections shall extend over any public right-of-way, except in the MU-1, MU-2, CCD and BUS zoning districts. Enclosed fire escapes, outside stairways, balconies, chimneys, flues or other projections shall not extend into any required yard. Open fire escapes may project as much as three feet into a required yard.
- b. Within the MU-1, MU-2, CCD and BUS districts, structural or ornamental features may project not more than six inches over the public right-of-way line, provided that no

projection shall extend beyond a vertical projection of the property line between the sidewalk or ground level and nine feet above such sidewalk or ground. Additionally, bay windows, porches, balconies and/or fire escapes (the "projection") may project not more than three feet over the public right-of-way line provided all of the following conditions are met:

1. The projection maintains a clear height above the sidewalk or ground level of the right-of-way of at least nine feet;
 2. The projection does not encroach upon the vision triangle as provided for in Article IX;
 3. The projection is at least four feet from an imaginary line drawn perpendicular to the face of the nearest curb;
 4. The person requesting the projection has received the written approval of the city and Gainesville regional utilities as regards the development plan; and
 5. The person requesting the projection has received the written approval of the city traffic engineer and public works department, the traffic engineer of the county or designee, the state department of transportation, and any other governmental agency having jurisdiction over the projection, as applicable.
- c. In any non-residential zoning district, marquees, canopies, or awnings may extend out 2/3 of the way between the face of a building and the curb, provided all of the following conditions are met:
1. There must be a sidewalk in front of a portion of the building where the marquee, canopy or awning is to be placed.
 2. The outside edge of the marquee, canopy or awning must be at least two feet from an imaginary line drawn perpendicular to the curb.
 3. The marquee, canopy or awning must maintain a clear height above the sidewalk of at least nine feet. No support for the marquee, canopy or awning may extend below this clear height.
 4. The person requesting the marquee, canopy or awning must receive written approval from both the city as regards and Gainesville regional utilities as regards the site plan.
 5. The person requesting the marquee, canopy or awning must receive written approval from the city traffic engineer and public works department, or, if the right-of-way is not city-owned from the government entity with jurisdiction over the right-of-way.
 6. The marquee, canopy or awning must not encroach into the vision triangle as provided by Article IX.
 7. The property owner shall be responsible for removing the marquee, canopy or awning at the property owner's expense upon notice that a road or right-of-way project requires it to be removed. If the property owner does not remove it, the governmental agency with jurisdiction over the right-of-way shall remove it and bill the property owner for the cost of removal.
 8. If the right-of-way belongs to the city, the property owner shall enter into a license agreement with the city indemnifying and holding harmless the city, its officers, agents, and employees, from any property damage, including loss, and any personal injury, including death, caused in any way by the projection of the marquee, canopy or awning over the right-of-way, and containing such other provisions as deemed necessary by the city attorney to protect the interests of the city. The license agreement shall be for a period of one year, and shall be renewable from year-to-year provided the marquee, canopy or awning is maintained in good condition and meets the standards set forth in this section.
- (4) Exceptions to height limits. Place of religious assembly spires, chimneys, water towers, transmitter towers, smoke stacks, flagpoles, monuments, television antennas and similar structures and their necessary mechanical appurtenances may, where permitted, be erected

above the height limits established in this chapter; however, the heights of these structures or appurtenances thereto shall not exceed the height limitations prescribed by the airport zoning ordinance in section 3-166 et seq.

(5) Standard minimum distance between buildings.

- (a) Definition. As used in this subsection, "building separation" shall mean the physical distance that buildings on the same lot or parcel are set apart from each other.
- (b) Single-family residential districts and other districts allowing single-family development. In a single-family residential district, as defined in section 30-51, there shall be no minimum separation between principal single-family buildings or between principal single-family buildings and accessory buildings, except as required by the standard building code. In all other zoning districts, there shall be no minimum separation between principal single-family buildings or between principal single-family buildings and accessory buildings, except as required by the standard building code.
- (c) Other zoning districts allowing residential units. In zoning districts not covered by paragraph (b) above but allowing residential units, there shall be a building separation of 15 feet between any two single-story principal buildings, 20 feet between any two-story principal building and a single-story or two-story principal building, and 30 feet between a principal building greater than two stories and any other principal building. There shall be no minimum building separation between the two non-residential principal buildings except as required by the standard building code.
- (d) Other districts. In all zoning districts not covered by paragraphs (b) and (c) above, there shall be no minimum building separation between two principal buildings, a principal building and an accessory building, and two accessory buildings, except as required by the standard building code.
- (e) Reduction in minimum building separation.
 - 1. Except as may be required by the standard building code, the appropriate reviewing board may grant a reduction in minimum building separation during the development plan review. When determining whether it will grant the reduction, the board shall consider the following criteria:
 - a. The required minimum building separation would not allow implementation of the densities allowed on the land.
 - b. The buildings are oriented so that the shortest edge of one building lies in a plan [plane] that is parallel to the plane of the longest side of the other building.
 - c. The buildings are designed and oriented so that they
 - i) allow space for outdoor uses customarily related to day-to-day residential activities;
 - ii) allow reasonable exposure to light and air circulation;
 - iii) will not result in sound resonating that would be a nuisance to residents;
 - iv) there is a view of the sky from all directions;
 - v) do not interfere with the reasonable security and privacy interests of residents.
 - 2. If development review is not required, a petition for a variance to reduce the minimum building separation may be submitted to the board of adjustment. The board of adjustment may grant the variance upon the same criteria as stated in paragraph (e) above.

(6) Sidewalks.

- a. Intent. The intent of this section is to enable pedestrian activity throughout the city, especially as a means to promote pedestrian and bus trips, pedestrian safety and

accessibility. Sidewalks are required in conjunction with development orders in every zoning district except zoning districts which implement the industrial land use category or as otherwise provided herein. This shall apply to all development orders issued after September 11, 2000, in every zoning district (except as otherwise provided herein), and on any parcel or lot where a roadway is existing adjacent to the proposed development or where there is a reasonable likelihood of mass transit service or a pedestrian need for sidewalks. Sidewalk(s) are required on all public and private streets, on both sides, except as specifically excluded or modified by this section.

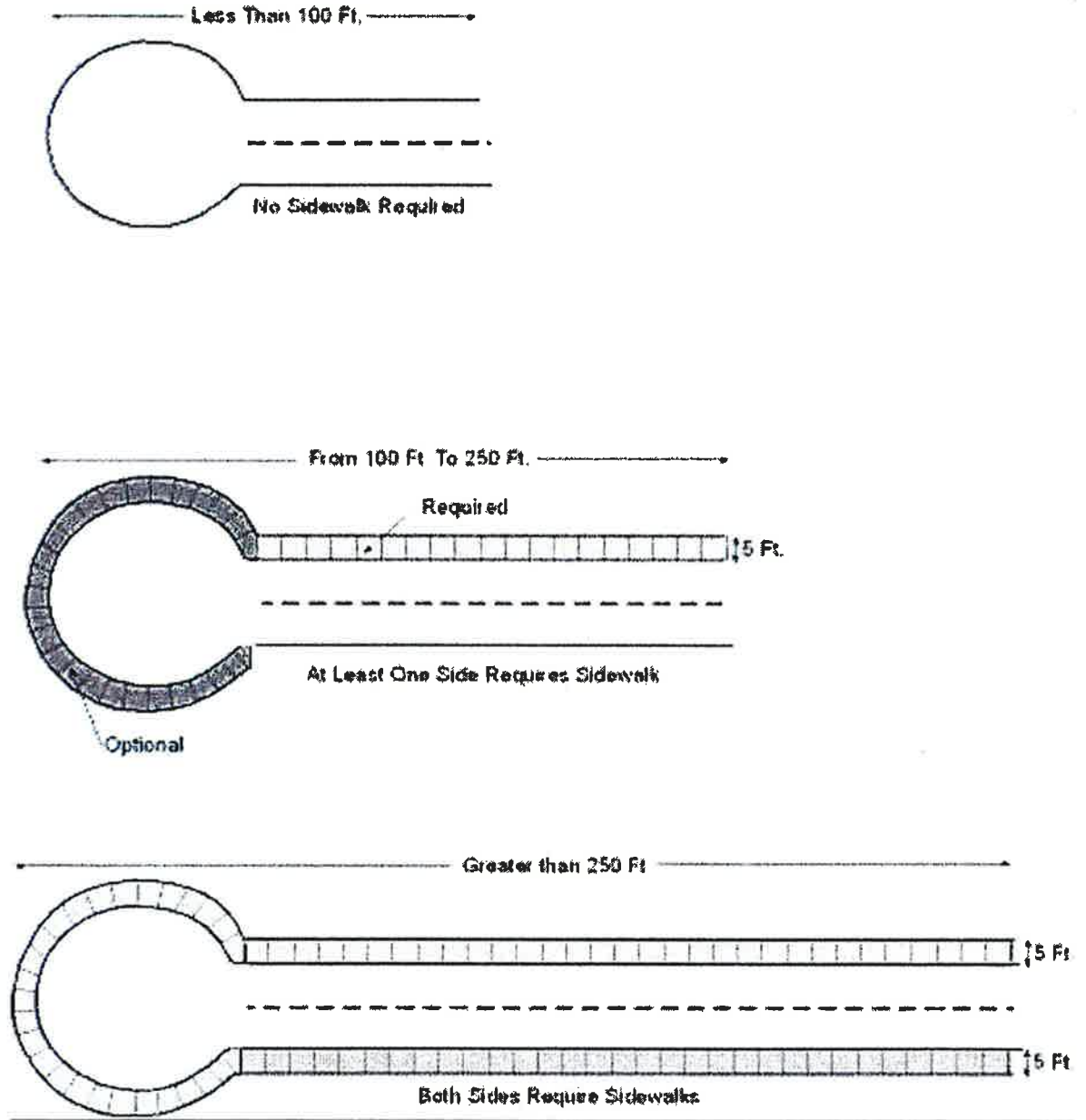
- b. General requirements. Where sidewalks are continuous, except for isolated lots or plots, in connection with the issuance of building permits, the city shall require an owner of an isolated lot or plot to provide sidewalks, except as provided below.

Sidewalks shall be constructed, if not already existing, along the street frontage of a lot which is being developed for a permitted use, except no sidewalks are required for:

1. Additions or renovations to, or new construction of single family homes on lots platted prior to January 1, 2003, unless a new single family home is proposed to be built on a lot where there is a sidewalk on the adjacent lot frontages.
 2. The addition or new construction of 500 square feet or less of gross floor area on any property or any paving of fewer than five parking spaces or equivalent area.
 3. On alleys providing a secondary access to a lot of record.
 4. Any lot on land designated as industrial on the future land use map of the city.
- c. Cul-de-sac or dead-end loop street(s) requirements. A sidewalk is required on at least one side of a street on a cul-de-sac or dead-end loop street(s) from 100 to 250 feet long, except as provided below. Sidewalks are required on both sides on cul-de-sac or dead-end or loop street(s) greater than 250 feet long. For a project in which the closest lots to a connecting street are at least 1,000 feet from the street it stems from, sidewalks are required on at least one side of the street up to the lot nearest the connecting street. Illustrations are shown below.

Exception: Sidewalks are not required on a cul-de-sac or dead-end or loop street(s) less than 100 feet long.

Cul-De-Sacs



Cul-De-Sacs

- d. Special area plan. Sidewalks shall meet the specific requirements of any special area plans any provisions of this Code to the contrary notwithstanding.

- e. Responsibility for construction. The installation of sidewalks is the responsibility of the developer/owner and the sidewalks shall be installed prior to the issuance of a certificate of occupancy by the city. For a phased development, sidewalk(s) shall be completed to serve any area for which a certificate of occupancy or any temporary occupancy is required. The developer/owner shall only be responsible for the sidewalk on the same side of the street(s) on which the approved plan is adjacent, except as specifically modified by paragraph h. below.
- f. Design standard. Sidewalk(s) and ramp(s) shall be constructed in accordance with the City of Gainesville Public Works' Design Manual. Whenever a sidewalk intersects a curbed street, ramps shall be installed to facilitate handicap access.
- g. Dedication. From time to time, an amendment to an existing developed area may require the installation of a sidewalk, however, inadequate right-of-way may exist to such extent that a developer/owner cannot install a sidewalk. In this event the city shall require the developer/owner to dedicate sufficient right-of-way to install proper sidewalk areas along all boundaries of the proposed development abutting public streets, or provide an easement to the homeowners association or condominium, as applicable, for additional common area for the sidewalk. The dedication of sufficient right-of-way for sidewalk improvements must be necessary to serve the development or reduce the impact of the development on the city's transportation system, and must, in all instances, meet the "essential nexus" and "rough proportionality" requirements established by the United States Supreme Court in the cases of *Nolan v. California Coastal Commission* and *Dolan v. City of Tigard*.
- h. Modifications. The appropriate reviewing board ("board"), city manager or designee (hereinafter "city manager") where no board approval is required, may approve modifications from the terms of this section. The board or city manager, may determine the appropriate location and/or termination of sidewalk(s), determine that dedication of right-of-way meets the required obligation, or determine that a portion of a sidewalk may be narrowed, as may be consistent with state accessibility requirements, or any combination of the above, as follows:

The board or city manager shall require the petitioner to provide information in the form of reports, maps, diagrams and similar material to support their request for modification. The board or city manager shall make their determination based on the following criteria:

The conditions and circumstances, which do not result from the actions of the developer, warrant modification of the sidewalk requirements. In reaching its decision, the board or city manager shall apply the following standards:

- i. The need to relocate utilities that must be moved solely in order to facilitate the construction of sidewalk;
- ii. Construction or redesign of stormwater management facilities;
- iii. Insufficient building setbacks;
- iv. A substantial lack of right-of-way;
- v. Protection of heritage and champion trees;
- vi. Excessive slope or other topographic or geological features;
- vii. Existing and expected future isolation of the subject parcel;
- viii. Sidewalks are not permitted by state or local agencies with jurisdiction over the subject right-of-way.

Attachment “E”

Applicant’s Waiver Request and Response to Comments

MEMORANDUM

To: Lawrence Calderon, Lead Planner 15-0490
From: Craig Brashier, AICP, Planning Manager
Date: May 17, 2016
RE: NW 43rd Street and NW 53rd Avenue Retail Development
Waiver / Modification Request Letter

The proposed project site is located within the City of Gainesville Transportation Mobility Program Area (TMPA) Zone B. The City of Gainesville Comprehensive Plan Mobility Element Objective 10.3 states that, "The City's Land Development Code shall provide design standards for all new developments and redevelopment within the TMPA." Policy 10.3.1 of the same comprehensive plan section states that, "The City shall use the Central Corridors Overlay District design standards in the Land Development Code for development / redevelopment projects within the TMPA."

The Special Area Plan for Central Corridors is provided in LDC Appendix A, Section 5. The build-to line intent and standard are provided in Paragraph (k). The intent of the build-to line is to define the relationship between the building façade and the street and streetside sidewalk. Over time, the intent is to pull building facades close to the street and streetside sidewalks. The required build-to line is 80 feet from the curb or edge of pavement.

LDC Appendix A, Section 5(k)(2) allows for variations to the build-to line standard. The appropriate reviewing board, city manager or designee may approve a greater *build-to line* (farther from the street) than the required *build-to line* when site constraints such as significant tree features or significant design features warrant it. If such approval by the appropriate reviewing board, city manager or designee is granted, the front yard area must be landscaped to provide shade for pedestrians with tree plantings and to establish the *street edge* articulation.

The applicant is requesting an increase to the build-to line along NW 43rd Street from 80 ft. to 93.8 ft. for the ±3,000 sq. ft. building and to 95.9 ft. for the ±2,300 sq. ft. building. The applicant is also requesting an increase to the build-to line along NW 53rd Ave. from 80 ft. to 92.2 ft. This increase is necessary because this site is unique in that the sidewalks are not located directly behind the roadway edge of pavement. There is a ±10 ft. gap between the sidewalk and the edge of pavement along NW 43rd Street and a ±20 ft. gap between the sidewalk and edge of pavement along NW 53rd Street. Therefore, the proposed building locations are consistent with the intent of the Central Corridors build-to line requirement by placing the building façades closer to the sidewalks and public realm, but do not meet the adopted standards since it is measured from the edge of pavement and there are significant gaps between the sidewalk and edge of pavement. Additionally, the building placements are consistent with the proposed 10 ft. to 100 ft. MU-1 front setback range in the City's LDC updates.

The Special Area Plan for Central Corridors also establishes an intent and standard for sidewalks in LDC Appendix A, Section 5(m). The stated intent is to provide the pedestrian with a pleasant, safe, and convenient place to walk, and mitigate traffic impacts by making the area more walkable. The minimum sidewalk width for Commercial / Institutional / Office / Mixed-Use along a collector roadway is 8 ft. Both of the adjacent NW 43rd Street and NW 53rd Avenue roadway segments are classified by the City of Gainesville as Collector roadways.

LDC Appendix A, Section 5(m)(2)c. allows the appropriate reviewing board, city manager or designee to modify the required sidewalk width to achieve the stated intent. **The applicant is requesting a reduction to the required minimum sidewalk width from 8 ft. to 6 ft.** The sidewalk width reduction is required to minimize impacts to existing utilities and stormwater management swales along the roadways. The requested 6 ft. wide sidewalks will achieve the stated intent by widening the existing sidewalks by 1 ft. to provide a pleasant and convenient place for pedestrians. The sidewalk will also maintain safety for pedestrians due to the separation from NW 43rd St. and NW 53rd Ave.

Due to the location of existing GRU utilities and utility easements along NW 53rd Ave. and a portion of NW 43rd St., the applicant is requesting modifications to the perimeter landscape requirements established in LDC Sec. 30-252(1). The proposed plans provide the required 9 ft. wide buffer, however the required shade trees cannot be planted within this buffer area due to the location of existing utilities and easements. Therefore, the applicant is requesting a modification from the planting requirements within the perimeter buffer area where there are conflicts with GRU utilities and utility easements.

As shown on the Landscape Plan, shrubs and sod are proposed in the areas where there are conflicts. Along NW 43rd St. where there are no conflicts, the required shade trees are provided. In order to demonstrate consistency with the intent of the City's landscape requirements, the applicant is adding extra landscape islands to the parking area along NW 53rd Ave. in order to provide the required number of street trees required by LDC Sec. 30-253.1.

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May 17, 2016

Lawrence Calderon, Lead Planner
City of Gainesville
Planning Department
306 NE 6th Avenue, Thomas Center B
Gainesville, FL 32601

Re: NW 43rd Street and NW 53rd Avenue Retail Development
PB-16-00050

Dear Lawrence:

Please find the following items included with this cover letter:

- Revised Traffic Impact Study;
- Revised Stormwater Report;
- Waiver / Modification Request Letter;
- Revised Development Plans;
- CDs containing PDFs of all documents; and
- GRU package containing all required items.

We submit these items along with this letter as a means to address your comments dated April 27, 2016.

Technical Review Committee:

Planning Comments (Approvable with conditions) Lawrence Calderon, Lead Planner, 334-5023

1. Please ensure that all uses proposed for the development are allowable uses within the MU-1 district.
Outdoor storage and sales are not allowed within any business district.
 - **All proposed uses are allowable within the MU-1 district.**
2. Please correct the data shown on Item 2 on the front sheet; it references University Ave and SW 1st Avenue.
 - **Item 2 has been corrected to reference the appropriate street frontages.**
3. Show the limit of SF boundary along the west boundary of the property.
 - **Limit of single family property boundary has been added. See Sheet C1.00.**
4. Please show the setback limits for development adjacent to a residential district.
 - **Setback limit is shown on Sheet C1.00.**
5. The proposed traffic activities at the SW corner of the site is within close proximity to the closest single-family dwelling to the west; please demonstrate how those potential impacts will be addressed. Compatibility issues must be addressed in the Special Use Permit; it appears that a hospital would generate additional emergency activities which may have impacts on the adjacent residential area.
 - **All improvements within the proposed development meet the residential setback requirements established within the MU-1 district.**

6. Here are the criteria for reviewing a Special Use Permit; please demonstrate how the development complies.
 Sec. 30-233. - Criteria for issuance.
 No special use permit shall be approved by the city plan board unless the following findings are made concerning the proposed special use:
- a. That the use or development complies with all required regulations and standards of this chapter and all other applicable regulations.
 - **Noted. This development complies with the referenced regulations.**
 - b. That the proposed use or development will have general compatibility and harmony with the uses and structures on adjacent and nearby properties.
 - **This development meets the residential setback requirements established within this district.**
 - c. That necessary public utilities are available to the proposed site and have adequate capacity to service the proposed use and development.
 - **Necessary public utilities are available to adequately serve this site.**
 - d. That the use or development is serviced by streets of adequate capacity to accommodate the traffic impacts of the proposed use.
 - **Per the Transportation Mobility Element from the Comprehensive Plan, the roadway LOS for all roadways in city limits shall be LOS E.**

The FDOT 2012 generalized service volume tables are used to determine the acceptable AADT. Table 1 – Generalized Annual Average Daily Volumes for Florida’s Urbanized Areas does not provide AADT for an LOS of E for State Signalized Arterials 40 mph or over, but does provide AADT for LOS of D which will be used for this study.

The daily trip generation estimated as provided in the traffic statement is provided below;

Hospital (610) - 10,820 SF = 143 VPD
 Drive-In Bank (912) – 2 Drive-Thru Lanes = 279 VPD
 Coffee Shop with Drive Thru (937) – 2,300 SF = 1,883 VPD

NW 53rd Avenue

An LOS of D equates to an AADT of 17,700 or lower for a 2 lane undivided state road. An additional 10% is reduced for Non-State signalized roadways giving an AADT of 15,930.

FDOT Traffic Online gives an AADT of 14,000 on NW 53rd Ave east of NW 35th Drive. Based on peak hour turning movement counts that were performed for the traffic study for this site, NW 53rd Ave east of NW 43rd St experiences more traffic than the segment west of NW 43rd St, so the segment west of NW 43rd St is expected to have a lower AADT than 14,000 vehicles per day and using this number provides a conservative estimate.

The trips generated on NW 53rd Avenue for the site are estimated to be $(143+279+1,883) \times (14,000/28,200) = 1,144$ VPD.

The total trips estimated at buildout is $14,000 + 1,144 = 15,144$ VPD, which is less than 15,930 AADT, so this site will not exceed the trip threshold of a LOS D or LOS E.

NW 43rd Street

An LOS of D equates to an AADT of 39,800 or higher for a 4 lane divided state road. An additional 10% is reduced for Non-State signalized roadways giving an AADT of 35,820.

FDOT Traffic Online gives an AADT of 14,200 on NW 43rd St just north of NW 53rd Avenue.

The trips generated on NW 43rd Street for the site are estimated to be $(143+279+1,883) \times (14,200/28,200) = 1,161$ VPD.

The total trips estimated at buildout is $14,200 + 1,161 = 15,361$ VPD, which is less than the AADT of 35,820 AADT, so this site will exceed the trip threshold of a LOS D or LOS E.

- e. That screening and buffers are proposed of such type, dimension and character to improve compatibility and harmony of the proposed use and structure with the uses and structures of adjacent and nearby properties.
 - **Necessary screening and buffers are proposed as required for this district.**
- f. That the use or development conforms with the general plans of the city as embodied in the city comprehensive plan.
 - **The development conforms with the general plans of the city.**
- g. That the proposed use or development meets the level of service standards adopted in the comprehensive plan and conforms with the concurrency management requirements of this chapter as specified in article III, division 2.
 - **The roadway LOS of the adjacent streets meets the required LOS of E per the comprehensive plan as explained in d above.**
7. The Development does not meet the build-to line; the line shown on your plans does not appear to match the required 80 foot line; please demonstrate how the line was determined. The situation is the same for both streets, 53 and 43rd.
 - **A modification to a maximum of 100' BTL is requested. This is in compliance with the proposed code and meets the intent of aligning adjacent block faces.**
8. The required sidewalk width along arterials is 8 feet; the existing sidewalks along 43rd and 53rd appear to be less than the required width.
 - **6' sidewalks are proposed to meet existing grading and drainage conditions of the site. A modification is requested.**
9. The sidewalks entering the property from 43rd Street can be more appropriately placed to facilitate pedestrian circulation.
 - **Sidewalks have been relocated as discussed in the TRC meeting.**
10. In order to facilitate pedestrian flow from the north, it seems that a sidewalk link should be constructed from the 43rd Street entrance to the sidewalk in front of Starbucks. The sidewalk off 43rd, closest to the intersection seems to run directly into the emergency vehicular drop-off area.
 - **Sidewalks have been relocated as discussed in the TRC meeting.**
11. It appears that there may be some issues with the bypass lane for the two drive-throughs.
 - **Drive-thru and bypass lanes will function separately and signage will be provided for smooth circulation throughout the development, as discussed in the TRC meeting. The lane nearest the bank, by-passes the bank.**
12. The dumpster at the entrance of the site from NW 43rd may not be the best location; please consider an alternate location.
 - **The angle of the dumpster has been revised as discussed in the TRC meeting.**

13. Is there an outdoor café associated with any of the developments, primarily the Starbucks?
 - **Outdoor café is proposed south of Starbucks, north of the proposed bank.**
14. The lighting associated with the ATMs appears to violate the light trespass requirement on the west boundary. I have some special concerns about the vertical illuminance for that site; I did not see the data.
 - **ATMs are specifically regulated to meet State and Federal guidelines. Refer to the updated E-1 sheet.**
15. We should discuss the height of fixtures and the automatic shut-off for that site due to its proximity to existing single-family residential. The SUP criteria will definitely come to play here and we should ensure that there are no negative impacts. I would strongly recommend lower light fixtures on the west side. It might be possible to use building light fixtures to do the trick there instead of using the full 30-foot poles.
 - **These areas are too far removed from the building to use wall mounted fixtures. Reduced mounting height to 25ft vs 30ft. Light trespass values are within allowable limits onto residential property with the exception of ATM lighting zones, see comment #14.**

Concurrency Comments (Approvable subject to below) Jason Simmons, Concurrency Planning, 334-5022

1. Please add the AM peak hour of adjacent street traffic calculations to the traffic generation information for the table in note #19 on the cover sheet.
 - **AM peak hour has been added to Note #19 on Sheet C0.00. AM trip generation for the peak hour of adjacent street traffic has been provided in the Traffic Statement.**
2. What is a "remote ED site?" Is this some type of hospital facility with beds for overnight stays by patients? Typically, the City uses the number of beds to determine the trip generation for this type of facility.
 - **This site is for emergency use and does not facilitate over-night stays therefore trips are based on square feet.**
3. Please correct note #19 to indicate that this project will meet the applicable provisions of Transportation Mobility Element Policy 10.1.4 and 10.1.6. The sentence referring to Policy 10.1.14 needs to be deleted because it is not applicable to this project since this is not a multiple-family development in the UF Context Area.
 - **Note #19 on Sheet C0.00 has been corrected.**
4. This development is located in Zone B of the Transportation Mobility Program Area and must meet all relevant Transportation Mobility Element Policy 10.1.4 and 10.1.6 criteria. Based on the estimated average daily trip generation, this development must meet 8 Transportation Mobility Element Policy 10.1.6 criteria.
 - **Noted. This development meets the Transportation Mobility Element Policy 10.1.6 criteria.**
5. It is not clear how the trip generation for the previous office building was calculated. General office (ITE 710) is 11.03 per 1,000 square feet of gross floor area and the daily trips would be 137 ($11.03/1,000 = 0.01103 \times 12,400 = 136.7$ or 137). The table in note #19 shows 269; please clarify.
 - **The equation $\ln(T) = 0.76 \ln(X) + 3.68$ was used to determine the trip generation of 269. This equation is expected to provide a more accurate estimate than using the average rate. The equations were also used for the AM and PM peak hour for General Office Building. An article titled "Guidelines for Using Trip Generation Rates or Equations" by ITE Technical Council Committee 6A-32, was followed to determine the appropriate trip generation calculation.**

6. Please revise the trip generation table on the cover sheet with the updated calculations.
 - **The table on Note #19 has been revised per the traffic study.**

7. This development is located in Zone B of the Transportation Mobility Program Area (TMPA) and must meet all relevant Transportation Mobility Element Policy 10.1.4 and 10.1.6 criteria. Based on the estimated average daily trip generation, this development must meet 8 Transportation Mobility Element Policy 10.1.6 criteria. Please revise note #19 on sheet C0.00 to say that this development is located in Zone B of the Transportation Mobility Program Area (TMPA) and must meet 8 Transportation Mobility Element Policy 10.1.6 criteria. Prior to receiving a final development order, this development must sign a TMPA Agreement for the provision of 8 Transportation Mobility Element Policy 10.1.6 criteria.
 - **Noted. This development meets the Transportation Mobility Element Policy 10.1.6 criteria. Please provide the TMPA fees and agreement for execution.**

8. How do the drive-through patrons of Starbucks get past the bank drive-through area? Is the lane closest to the bank on the west side not a drive-through lane for the bank?
 - **Drive-thru and bypass lanes will function separately and signage will be provided for smooth circulation throughout the development, as discussed in the TRC meeting. This lane bypasses the bank.**

9. In accordance with Policy 10.5.5 of the Transportation Mobility Element, 65-gallon sized trees are to be planted for the required landscaping along roadways listed in the annual level of service report produced by the North Central Florida Regional Planning Council within Zone B of the TMPA. Redevelopment sites must plant 50% of the number of street trees otherwise required by the Land Development Code. The trees shown on the landscape plan to be planted along NW 43rd Street and NW 53rd Avenue are only 30-gallon in size. Please replace these trees with 65-gallon sized trees. The site must also meet the automated irrigation system requirement.
 - **The street trees along NW 43rd Street and NW 53rd Avenue have been upsized to the required 65-gallon size. Please see notes on the landscape plan for information regarding irrigation.**

Public Works Department Comments (APPROVABLE - Subject to Comments)

Roadway & Site Design: COMMENTS - Reviewed by: Rick Melzer (352) 393-8407

1. The drive-thru lane for the Starbuck's appears to be shared with a drive-thru lane for the bank. Please clarify.
 - **Drive-thru and bypass lanes will function separately and signage will be provided for smooth circulation throughout the development, as discussed in the TRC meeting.**

2. The required by-pass lane for the Starbuck's drive-thru and the required by-pass lane for the bank can be blocked if stacking for the 2 bank drive-thru lanes exceeds the storage provided.
 - **The drive-thru lanes for the bank have been revised to allow a minimum 5 car stacking.**

3. The proposed dumpster location nearest NW 43rd St. will block the driveway entrance when garbage trucks are servicing the dumpster. Please revise.
 - **The angle of the dumpster has been revised as discussed in the TRC meeting.**

4. Please clarify with arrows the traffic circulation patterns for the drop-off area on the east side of the medical building. "Do Not Enter" signs may be required.
 - **Traffic arrows have been revised and a "Do Not Enter" sign has been added.**

Stormwater Management: COMMENTS - Reviewed by: Andrew Roberts (352) 393-8408

1. Where does the stormwater collected from inlets S-1 and S-2 being treated?
 - **Storm network has been revised and collection is provided. See Sheets C2.00, C2.01, C2.02, and C2.30.**
2. The post-developed discharge rate exceeds that of the pre-developed amount for the 100 year, 2 hour and 100 year, 4 hour storm events.
 - **Cumulative post-developed discharge rates are now below the pre-developed rates.**

Transportation Planning: COMMENTS – Reviewed by: Scott Wright

1. What is the purpose of the 2nd sidewalk connection from NW 43rd St? It seems like it will direct pedestrians into the drive-through area. It would be better to shift further north to align with the loading area for the HC spaces and the front sidewalk of the medical building.
 - **Sidewalks have been relocated as discussed in the TRC meeting.**

Transit: COMMENTS - Reviewed by: Matt Muller (352) 393-7820

1. Please consider applying any required developer contributions under Transportation Mobility Element Policy 10.1.6 to accessibility and amenity improvements at nearby bus stops.
 - **Noted.**

Solid Waste: COMMENTS – Reviewed by Steve Joplin

1. All three dumpster enclosures should be at least 10'x20' to accommodate recycling. Enclosure for southernmost building should be reoriented so it can be serviced from the other direction like the other two enclosures.
 - **All dumpster enclosures are now 10'x20'.**

Inspections: COMMENTS – Reviewed by Matt Williams

1. Typical asphalt design could not be located.
 - **See Sheet C0.12 for detail.**
2. The western elevation of the proposed bank appears to show an awning that based upon rough scaling appears to possibly encroach into the drive thru lane. Please verify horizontal and vertical clearance requirements are met.
 - **Overhang and doors have been added to the plans and are not in conflict. See Sheet C1.00.**
3. The floor plan for the bank shows a door on the western side and egress. Based upon scaling it appears the door will encroach into the drive thru lane. Please verify egress and clearance requirements are met.
 - **Overhang and doors have been added to the plans and are not in conflict. See Sheet C1.00.**
4. What is the anticipated traffic pattern for the drop off area / front door for the emergency? Vehicles depicted would seem to be entering / exiting in the wrong direction based upon parking lot aisle layout and typical passenger unloading.
 - **Traffic arrows have been revised and a “Do Not Enter” sign has been added.**

Building Department Comments Linda Patrick, Plans Examiner (PX934), 334-5050
patricklr@cityofgainesville.org

Fire and Life Safety Services (Approvable with comments) Tom Burgett, Fire Inspector, 334-5065

1. Please indicate the approximate location of the features of fire protection, including: --Fire department connection: Fire department connections shall be located at the nearest point of fire department apparatus accessibility or at a location approved by the authority having jurisdiction. (NFPA 13-8.17.2.4.6) -- Post Indicator Valve and/or Backflow Preventer for each building: Every connection from the private fire service main to a building shall be provided with a listed indicating valve located so as to control all sources of water supply. The post indicator valve shall be located not less than 40 ft. from the building, or as close to 40 ft. as is possible considering site limitations. (NFPA 24- 6.2) -- Fire Sprinkler and/or Standpipe main --Sprinkler Room [Gainesville Land Development Code Section 30-157(d) (15)].
 - **Backflow preventer with exposed OS and Y for the proposed medical building are shown on Sheet C3.00.**

Urban Forestry Comments (Approvable with comments) Earline Luhrman, Urban Forestry Inspector, 393-8188, 4/15/16

1. There are ten (10) heritage Red Cedar trees that will require inch-for-inch replacement on site, off site (within city limits) or a donation to the city's tree fund. These inches may be counted toward the landscape requirements.
 - **A mitigation table has been added to the landscape plans.**
2. There are two high quality heritage Live oak trees (24" & 26") will need the tree appraised value to be applied for their removals. These trees are a payment only and the fee is due at the time the development order or a site work permit is issued for the development. (24"= \$9,947.52 & 26"=\$11,674.52)
 - **A mitigation table has been added to the landscape plans.**
3. There are thirteen (13) regulated trees marked out for removal and this requires a 2:1 replacement on site, off site (within city limits) or a fee provided to the tree fund.
 - **A mitigation table has been added to the landscape plans.**
4. Along the western property line please indicate tree barriers to protect the trees during development.
 - **Silt fence is placed on the east side of the trees as a barrier. Please see Sheet C0.21.**
5. Palms do not count towards landscaping requirements but may be utilized as foundation plantings.
 - **Noted.**
6. Continue the street trees heading north on NW 43rd Street to the end of the property.
 - **Due to the GRU utility easement, street trees cannot be continued through to the northern end of the property.**
7. Make certain street trees are provided on both sides of the vehicular use areas.
 - **Vehicular use areas have been screened from adjacent properties and roadways as outlined within the City of Gainesville's Land Development Code, Sec. 30-252: "All vehicular use areas shall be separated by a perimeter landscaped area, a minimum of nine feet in width, from any public or private street and from any adjacent properties." According to code, "This landscape area is not required...When the paved ground surface area is completely screened from adjacent properties or streets by intervening buildings or structures."**
8. The palms along the western property line needs to be replaced with shade trees. (sheet LS-1)
 - **The palms have been replaced, as requested.**

9. Street trees are required along NW 53rd Street and if issues with the utilities there are methods in the code that are standards provided by GRU so street trees can be proposed for this development.
 - **As the utilities on 43rd and 53rd are existing, it is not possible to implement any of the methods outlined in the code. However, the parking lot has been modified so that the required street trees can be provided within landscape islands (outside of the existing GRU utility easement).**
10. On the south side in the landscape island add a shade tree in the sod area within the landscape island. (near entrance drive)
 - **Due to the presence of an existing GRU easement and utilities, a shade tree is not able to be placed in that particular landscape island.**
11. On the north side of the building add shade trees in the sod areas in front of the building (both sides).
 - **Shade trees have been provided in this area, as requested.**
12. Please add a shade tree behind or beside the dumpster pad.
 - **A shade tree has been provided by the dumpster pad.**
13. Please replace the Crape myrtles with Little Gem Magnolias trees. (Sheet LS-2)
 - **Crape myrtles have been replaced with Magnolia trees, as requested.**

GPD Crime Prevention Unit Comments, Dr. Richard Schneider, rschnei@ufl.edu

1. Parking light level approvable.
 - **Noted.**

If you have any questions, please contact me directly at (352) 331-1976 or hollys@chw-inc.com.

Sincerely,
CHW



Holly Simon, E.I.
Project Manager

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